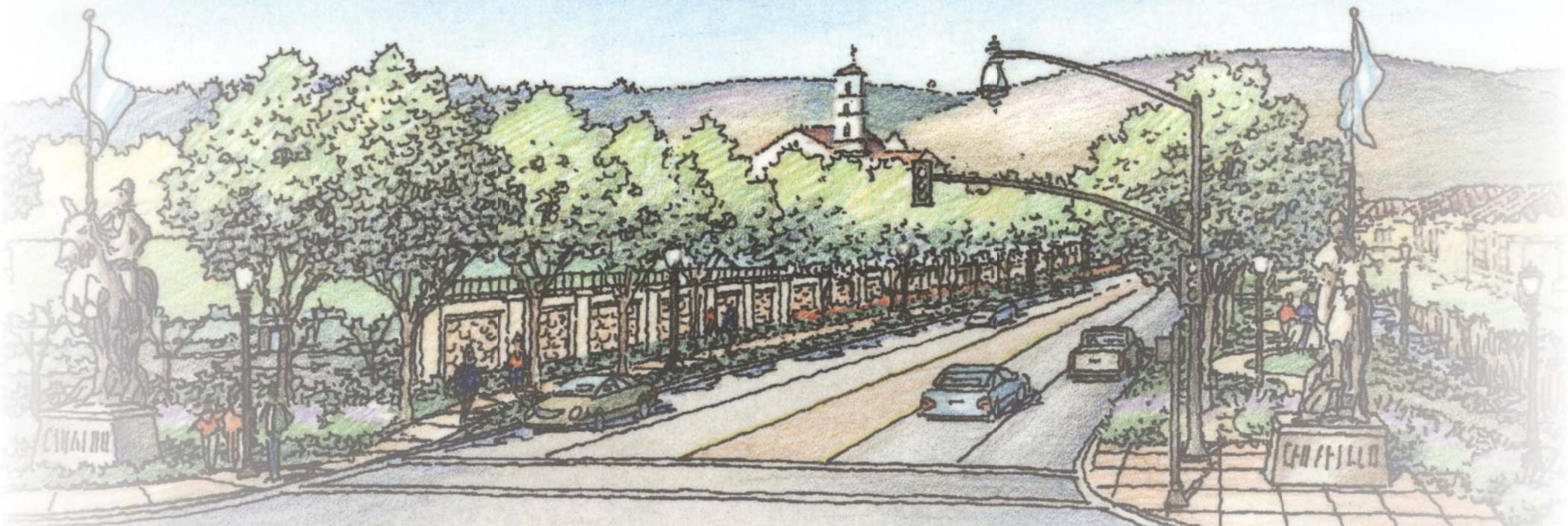


DAWSON DRIVE INDUSTRIAL AREA CONCEPTS & DESIGN GUIDELINES



City of Camarillo

May 12, 2010

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SECTION

1

INTRODUCTION and PURPOSE

Over the past decade, the City of Camarillo and its redevelopment agency have undertaken a series of revitalization efforts to enhance central Camarillo. In Old Town Camarillo, public and private investments along Ventura Boulevard have rejuvenated the downtown, creating an inviting, pedestrian oriented commercial district that is home to many of the city's restaurants and specialty retailers. Transit improvements along the Union Pacific railroad corridor have provided transit access via Amtrak and MetroLink from central Camarillo to cities across California. Lewis Road has been realigned and improved, providing more convenient vehicular access to Old Town. In addition, new infill development to the east has created hundreds of homes and a broad variety of businesses, most notably in the mixed use development, Village at the Park.

The approximately 180-acre Dawson Drive Industrial Area is located to the east of Old Town, sandwiched in between Lewis Road and the residential neighborhoods of Calleguas Gardens and Village at the Park. It houses many of the city's industrial businesses, in addition to large parcels of vacant and underutilized land, such as the Imation and EJM properties. While centrally located, the area is not easily accessible by vehicle, foot or bicycle. Lewis Road, Highway 101, Dawson Drive and the railroad tracks criss-cross the area, making it very difficult to navigate, particularly from east to west.

The purpose of the Dawson Drive Industrial Area Concepts and Design Guidelines is to articulate a vision for the revitalization of this area and establish a set of design standards and improvements that will guide its redevelopment into a vibrant district. A key component of the plan is to enhance signage, circulation and access so that this area becomes better integrated into central Camarillo. As implemented over time, the Concepts and Design Guidelines will encourage property owners, residents and businesses to improve existing properties and create new infill development that will help unify the area and connect central Camarillo.



FIGURE 1-1
Vicinity Map

The California State University Channel Islands campus community, established in 2002, lies 1.3 miles to the south of the City of Camarillo and the Dawson Drive Industrial Area. Primary access to the site is provided via Lewis Road (State Route 34) both from the north and south. The campus currently includes 634 acres. Residential, town center, open space and school land uses have been developed around road patterns and pedestrian/bicycle linkages. The residential neighborhoods with the highest density have been located toward the Academic Core and related facilities in order to offer the greatest walking convenience to the highest concentration of residents.

REPORT OVERVIEW

This report begins with a brief overview of the Dawson Drive Industrial Area location, describing the opportunities and constraints of its four subareas. It then presents a set of revitalization objectives, proposes potential changes in land use by subarea, and identifies key opportunity sites that can catalyze future infill development. It also presents a set of building and design guidelines that will encourage a coherent sense of place.

In order to achieve revitalization of the area, a series of infrastructure and circulation improvements are needed. Each of these improvements are described and mapped by subarea. Along with these capital improvements, Camarillo needs to implement a series of economic development and housing programs that will encour-

age businesses and residents to invest in the area. The report concludes with an implementation strategy that describes proposed actions to be undertaken and most likely funding sources that the City and Agency can utilize to accomplish the objectives of the Dawson Drive Industrial Area Concepts and Design Guidelines.



FIGURE 1-2 Sub-Areas

DAWSON DRIVE SUB-AREAS

The Dawson Drive Industrial Area has four distinct sub-areas:

North Dawson Drive

This is the oldest sub-area with the original development occurring before World War II continuing through the 1960s. For the most part the area’s development was dependent on the railroad. Many of its buildings continue to function for their intended use but are in need of substantial repair with several properties in violation of current city codes. General maintenance of the area is substandard and the basic streetscape is nonexistent or, where it exists, is substandard. Many of the parcels are small, shallow, and/or irregular, as a result of the railroad tracks and major roads that run through the area and lot subdivisions that preceded modern planning and zoning codes. Views of this area from the railroad need to be enhanced.



North Dawson Drive looking north from the Lewis Bridge. Note the poor condition of the buildings, overhead utilities, outdoor storage, shallow lots, lack of sidewalks, and the unsightly view into the area from the railroad.

South Dawson Drive

This area was developed during the 50s and 60s to service light industrial uses. The building stock is generally functional and in good repair but some buildings could benefit from façade enhancements, screening and landscaping. Generally, the roadway is adequate, but two portions of the road lack sidewalks. The businesses in the area are generally sound and provide a mix of tenants not found anywhere else in the City. The rents in the area are modest which allows startups and businesses that require low overhead to prosper. The Community Development Commission (City Council) has indicated that the South Dawson businesses are to be maintained and supported.



South Dawson Drive. Note the overhead wires, modest buildings, and minimal landscape.



South Dawson. Note the unsecured outdoor storage.



Constitution Avenue. Note the lush landscaping and more contemporary buildings looking north.



Calle San Pablo. Note the landscaping and building condition.

Constitution Avenue and Calle San Pablo Industrial Areas

These areas were developed during the 80s and 90s as light industrial/flex space. Many of the buildings have offices facing the street and roll-up loading doors facing rear driveways. The structures are in very good condition and the area has lush landscaping. On the negative side, these areas are automobile-oriented and the streets have no sidewalks and no pedestrian-scale lighting.

Imation / EJM Properties

These properties are suitable for development / redevelopment. The Imation property consists of 27.6 acres and is the location of the former Imation plant, a magnetic tape manufacturing facility that closed in 2008 and sits vacant. The EJM property, approximately 23.5 acres, is currently in agricultural use, yet surrounded on three sides by industrial uses and is currently zoned for light industrial. EJM Development Company bought the property in 2000. They build and hold high end industrial for 5,000 to 50,000 square foot users that tend to need office, back office, and production areas combined with full loading docks.



EJM property looking north to the Imation property.

CONSTRAINTS ON REDEVELOPMENT

Various features of the Dawson Drive Industrial Area and its subareas have prevented the area from developing or redeveloping on its own. The North Dawson Drive area is particularly challenging, due to its small and irregular lots and old and obsolete buildings. These physical characteristics limit the type of businesses that can locate there and thus diminish the economic potential of the area. The existing businesses generate low rents, and as a result property owners have little incentive or ability to invest in the upkeep and improvement of their properties. Poor building repair and code violations lead to a negative cycle of deferred maintenance, low rents, and further disinvestments that have contributed to the current deteriorated physical condition of the area.

Another significant constraint affecting both North Dawson and the remainder of the Dawson Drive Industrial Area is the lack of access to the area from other parts of Camarillo. The Union Pacific railroad tracks cut off the area from Old Town, and the configuration of Lewis Road, Dawson Drive, Petit Street and Highway 101 make the area difficult to navigate. A lack of signage throughout the area compounds the current access and circulation problems. The combination of these factors means that the Dawson Drive Industrial Area lacks a sense of place to draw in businesses and encourage development in the area.

PUBLIC PROCESS

Mogavero Notestine Associates and Seifel Consulting conducted several stakeholder interviews during the day on April 15th. Stakeholder interviews allowed the consultant team to learn vital information about community concerns, past efforts and future hopes. Stakeholders included representatives from:

- Dawson Drive business community
- California State University, Channel Islands
- Chamber of Commerce
- Property Owners

The public part of the program began with a workshop, during the evening of the 15th, with the Calleguas Gardens neighborhood. About 25 people took part in this workshop. The neighborhood workshop was followed by a general public Workshop, during the evening of April 16th, which was attended by approximately 30 members of the broader community, as well as members of the Citizens Advisory Committee of the Redevelopment Agency.

Mogavero Notestine Associates conducted a Community Image Survey at the beginning of both workshops. The surveys consisted of 40 slides of photographs taken in Camarillo and other communities. As a whole, the images presented contrasting views of a neighborhood — streets, sidewalks, retail potential, office buildings, housing, gateways and transportation features. The surveys were used during the workshops to involve participants in the planning process and to gauge what they wanted to see in the area.

Group sessions followed and involved area-wide planning effort. Participants were divided into working groups and given several minutes to respond to a series of four questions in one or two brief thoughts with three- or four-word statements for each question. The balance of the time was spent reaching consensus and prioritizing the list. Responses were written down on large print pads and the top two to three concepts for each question were presented to all participants at the Plenary Session (a session attended by all of the participants). The workshop concluded with a general discussion.



SECTION 2

A VISION FOR REVITALIZATION

The revitalization of the Dawson Drive Industrial Area is a critical component to enhancing and redeveloping central Camarillo. As described above, the Dawson Drive Industrial Area suffers from a variety of physical constraints and haphazard development patterns. Although located in the heart of Camarillo, it is not easily accessible by vehicle, foot or bicycle from surrounding areas. The existing street patterns and the lack of signage and gateway treatments makes it difficult to circulate through many parts of the area. Many of the existing properties are vacant or underutilized, and particularly along Dawson Drive, many of the buildings are substandard and/or deteriorated.

The revitalization objectives of the Dawson Drive Industrial Area Concepts and Design Guidelines are to:

- Strengthen the Dawson Drive Industrial Area so it can better serve local residents and businesses, as well as attract new visitors to central Camarillo
- Create a sense of place, capitalizing on its central location, transit access and history
- Enhance existing neighborhood fabric
- Improve access, signage and visibility
- Upgrade vehicular, pedestrian and bicycle circulation to more seamlessly connect the Dawson Drive Area with other key parts of central Camarillo
- Promote transit use and access along the North Dawson Drive rail corridor
- Encourage infill development and redevelopment of properties
- Expand private investment and business growth

In order to accomplish these objectives, the Guidelines recommend five types of improvements:

- Circulation and Transit- vehicular, pedestrian, bicycle and transit improvements
- Placemaking- enhancing the area through improved land use, design and planning guidelines, signage, gateway treatments, and landscaping
- Infrastructure- utility improvements to encourage infill development
- Economic development- programs to retain, expand and attract new businesses
- Neighborhood enhancement- programs to enhance neighborhood fabric and encourage redevelopment and infill development of properties

The following sections describe the first three of these proposed improvements, as these are the physical improvements that will set the stage for the revitalization. The economic development and housing programs are described in the implementation strategy at the conclusion of the report.

SUB-AREA CONCEPTS

As previously indicated, the Dawson Drive area can be divided into four sub-areas which include North Dawson Drive, South Dawson Drive, Constitution Avenue and Calle San Pablo Industrial Areas, and the Imation / EJM Properties. Each of these sub-areas will undergo differing changes:

North Dawson Drive will see the most dramatic change with new streetscape enhancements, landscape improvements, organized parking facilities, infill and redevelopment of underutilized properties, revitalization along the railroad, and improved pedestrian and bicycle connection to the Metrolink Station and Camarillo's "Old Town". There will be a mixture of office, retail, restaurant and the possibility of mixed uses developed in a theme reminiscent of a turn of the century railroading environment. Redevelopment of North Dawson will also take advantage of the subarea's proximity to Old Town and the Metrolink Station, as well as its relationship to California State University Channel Islands (the subarea is the first activated commercial district encountered as you enter Camarillo from the South via Lewis Road).

Existing light-industrial uses will eventually relocate south of the Lewis Road Bridge.

The photographs below are examples of industrial structures that have been rehabilitated and converted to contemporary commercial uses.



Bakery in Santa Cruz, CA



Produce Market in Loomis, CA

Building styles reminiscent of railroad themed buildings provide a design precedent for the revitalization to come.



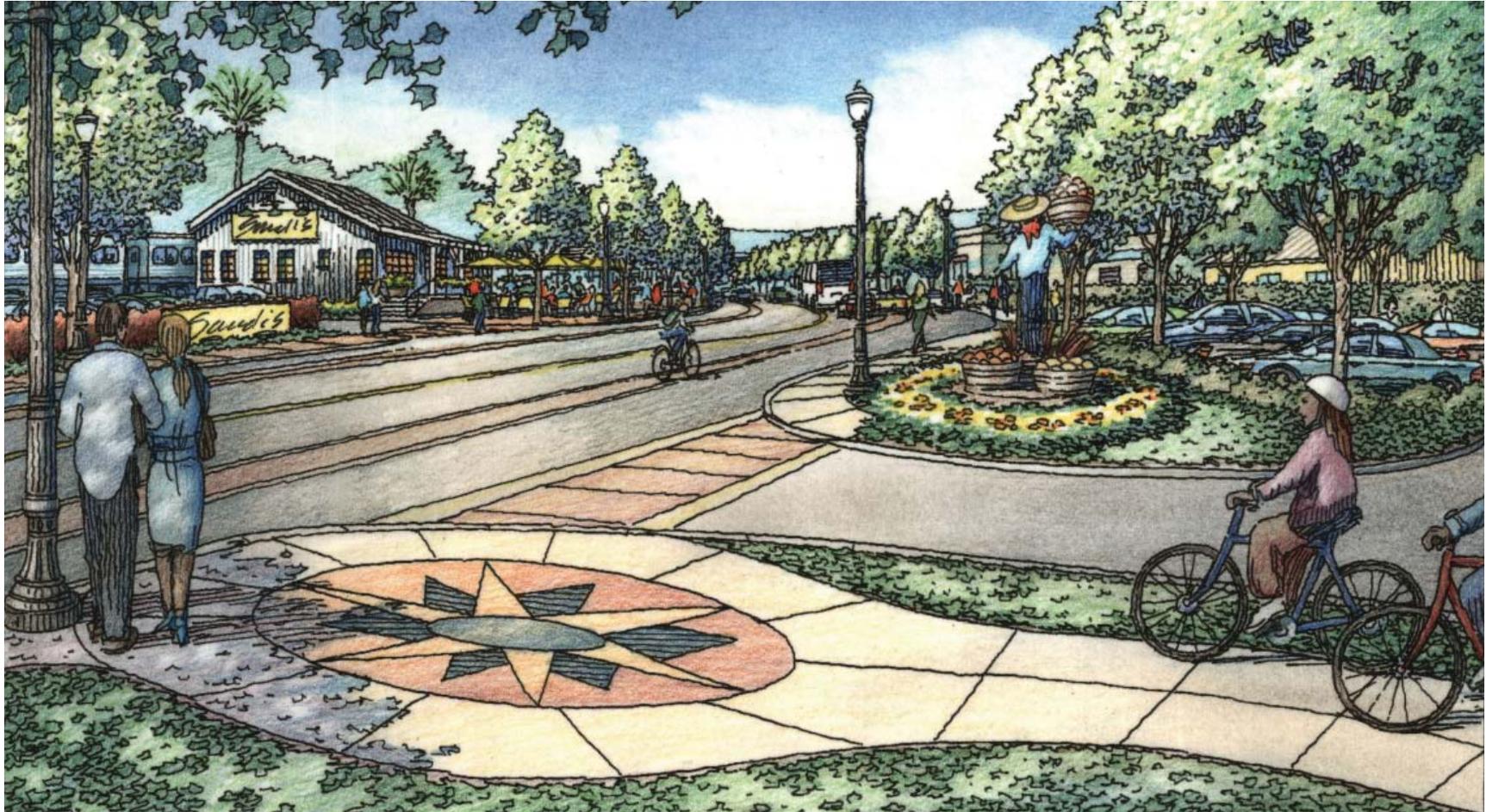


FIGURE 2-1
Looking North from the Lewis Road Overpass on Dawson Drive into the Historic Railroad Community



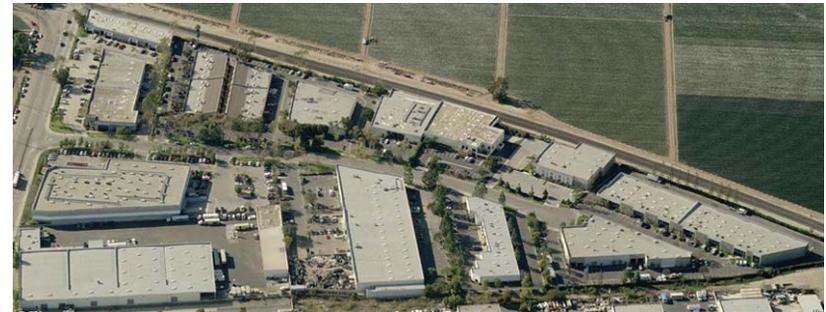
FIGURE 2-2
Looking at Proposed Metrolink Station from Old Town with Historic Railroad Community beyond

South Dawson Drive will remain a thriving light-industrial area. Its buildings will be rehabilitated over time. Utilities will be located underground and landscaping and minor streetscape enhancements will be installed. It is proposed that a screen wall will be installed along Lewis Road to enhance the view of the area and improve security. With certainty and the understanding of their importance to the City, businesses will be encouraged to remain and prosper through rezoning or the development of an overlay zoning designation that recognizes existing uses and development patterns.



A prototypical screen wall that could be constructed along Lewis Road to screen and provide security for South Dawson businesses.

The Constitution Avenue and Calle San Pablo industrial areas, because of the good quality of the existing building stock and environment, will, from a physical perspective, stay about the same. Over time some individual properties may be improved. With certainty and the understanding of their importance to the City, businesses will be encouraged to remain and prosper.



Calle San Pablo industrial area.



Constitution Avenue industrial area.

The Imation / EJM properties, because of highway access and future market forces, these areas will undergo a significant transformation into a thriving business park through a major private development / redevelopment effort. While not precluding development with uses permitted by the M-1 zoning, the properties will have the ability to act in concert with the City and University, to recruit business that relate to the assets of being near a University Campus. Lewis Road will be improved to include a parkway with a greenbelt incorporating an off-road pedestrian and bicycle trail and a gateway element will be located in the intersection of Lewis Road and Pleasant Valley Road.



Industrial / Business Park in Camarillo that could be a prototype for the development type and quality that is envisioned for future development of the Imation / EJM properties.

The proposed improvements to the sub-areas are discussed in more detail in subsequent sections of this document.



A prototypical Gateway feature that could be located at Lewis Road and Pleasant Valley Road.



FIGURE 2-3
Looking at the Gateway Element, Dawson Area Screening, and EJM Development Site at the Intersection of Lewis Road
and Pleasant Valley Road

SECTION 3

GENERAL PLAN and EXISTING LAND USE

The city's General Plan Land Use Element designates all of the area for industrial development, with the exception of the adjacent Calleguas Gardens Tract, which is shown for low-density residential (5 dwelling units per acre maximum). All of the study area is within the Camarillo Corridor Redevelopment Project Area.

The city's General Plan Circulation Element identifies Lewis Road/State Route 34 as a primary arterial, while Dawson Drive and Petit Street are industrial collector streets. Calle San Pablo and Constitution Avenue are identified as local streets serving industrial neighborhoods.

Typical of most light industrial areas the Dawson Drive area has a variety of differing existing land uses. As recently surveyed by the City of Camarillo, they include:

- Auto Service – 32
- Contractor – 3
- Designer – 1
- Manufacturer – 81
- Office – 11
- Places of Worship – 3
- Research and Development – 8
- Recreation – 8
- School – 1
- Service / Distribution – 26
- Service / Commercial – 3
- Storage – 8
- Truck Terminal – 3

In addition to these uses there are 57 vacant tenant spaces.



Village at the Park is located to the east of the project area.



Camarillo Old Town is located to the west of the project area.

Land uses surrounding the study area are varied. To the north, across the U.S. 101/Ventura Freeway, are developed industrial uses to the east of Lewis Road in the M-1 (Light Manufacturing) and M-2 (General Manufacturing) Zones. This area is currently being studied for a potential transit-oriented development project known as Sandstone.

South of the project area is vacant agricultural land in the unincorporated area of Ventura County. To the east is the Village at the Park project, which contains a mix of residential, commercial, and recreational uses. The properties to the west are vacant agricultural land, as well as the Camarillo Old Town area, which includes residential apartments in the RPD (Residential Planned Development) Zone, the St. Mary Magdalen Chapel in the RE (Rural Exclusive) Zone, and commercial uses in the COT (Camarillo Old Town) Zone.

SECTION 4

DESIRED LAND USES

The Concept for the Dawson Drive Industrial Area suggests land uses that the City would like to see and encourage to locate in the area. The suggested land uses (see Figure 4-1) would enhance the vitality of the industrial/mixed use area while preserving the character of the existing Calleguas Gardens neighborhood.

North Dawson - Historic Railroad

Taking advantage of its proximity to Old Town, the Amtrak and Metrolink Station, and its relationship to California State University Channel Islands, the Historic Railroad area of North Dawson (east of the railroad tracks and west of Dawson Drive) would cater to both nearby residents and commuters. The area could contain uses such as restaurants, fresh produce/local food markets, nurseries, flower shops, galleries, service offices, and other small scale retailers. Uses that require outdoor storage (other than nurseries) or uses that are auto related would be discouraged. The Historic Railroad refers as much to the design concept as anything else with the design style reminiscent of the former railroad depot building once located in this area. To allow these uses in the area will require rezoning, possibly to the Camarillo Old Town (COT) or Service Commercial (SC) zoning classification or retention of the M-1 zoning with its own overlay.

North Dawson - Mixed Use

The Mixed Use area is also located in the North Dawson sub-area but east of Dawson Drive. To support the Railroad Retro area and buffer the Calleguas Gardens neighborhood, the preferred land use would be mixed. This area could be home to a combination of uses such as small scale retail, restaurants, office, artist live-work, service commercial, and mixed use residential. Uses that require outdoor storage or are auto related would be discouraged. In order to allow these uses in the area a modification to the zoning will be required consistent with the adjacent Historic Railroad zoning. Design styles would be influenced by the Heritage Zone, which are those areas within 500 feet of the freeway and 1,000 feet of freeway interchanges that require a particular design theme. Design elements (consistent with Heritage Zone) such as those suggested for the Historic Railroad area (see Section 8 Design Guidelines page 75) would also be appropriate in the North Dawson - Mixed Use Area.

Constitution & Calle San Pablo - Light Industrial

Because these are fully functional light industrial areas, no specific uses are suggested other than those permitted or conditionally permitted in the



FIGURE 4-1 Preferred Land Uses with Edge Uses

Light Industrial (M1) classification in the City’s Zoning Ordinance. No unique design style is suggested.

Imation - Industrial Park

To improve the quality and composition of uses in the Dawson Drive area it is desired that the reuse of the Imation site is consistent with any higher quality business parks found in Camarillo. Uses such as offices, banks, restaurants, and service uses would be encouraged. No unique design style is suggested. As a result of the Dawson Drive Industrial Area Design Guidelines, the Calleguas Gardens neighborhood will be protected from future development.

EJM - Industrial Park / University Serving

As with the Imation site, quality use and composition of development are important factors. While the M-1 zoning will allow for this area to be developed with high quality industrial uses, future development could also be encouraged that would take advantage of the potential spin off from the University as it matures, it is preferred that the EJM site, when developed, provides a home for uses that support the California State University Channel Islands in a quality business or industrial park setting. Uses such as offices, banks, restaurants, and support commercial would be encouraged. No unique design style is suggested.

South Dawson - Light Industrial Overlay

No specific uses are suggested other than those permitted or conditionally permitted by the City’s Zoning Ordinance under the Light Industrial (M1) zoning classification and no unique design style is suggested. The purpose of creating an Overlay for the area is to allow the City to be flexible in interpreting development standards, and to support existing uses and development patterns (older developments may not meet all current standards) in an effort to encourage private enhancement efforts.

SECTION 5

CIRCULATION IMPROVEMENTS

This section describes and illustrates the recommended roadway widths and landscape treatments for the existing circulation network serving the Dawson Drive area, including existing and future roadways, pedestrian and bicycle facilities, and improvements to enhance access to transit.

Graphic cross sections are followed by descriptions of each of the roadways proposed for change. Future improvements may include the extension of Westpark Court into the area for limited vehicle and pedestrian access/circulation. Constitution Avenue and Calle San Pablo function well and no modifications are proposed.



PETIT STREET

Petit Street is the major entry into the Dawson Drive Industrial Area from east bound 101/Village at the Park Drive and connects Village at the Park Drive with North Dawson Drive, a portal to the planning area, as well as the Flynn Road Industrial Area. The suggested Section for Petit Street includes a 10' wide walkway / landscape area (with street trees/planting parkway), 6' bike lanes, 8' parking lane (south side only – the parking needed for any development on the north side of Petit can be accommodated on site) 12' travel lanes and a 14' median / turn lane.

The widening of Petit Street to accommodate the City's proposed configuration of 102' will require the acquisition of additional right-of-way (ROW). The analytical

assessment of the acquisition of additional ROW is beyond the scope of this study. Intuitively, acquiring ROW north of Petit Street would result in the removal of residential units that are of a lesser quality than those to the south, as well as property (northeast corner of Petit Street and Dawson Drive) that could be more intensely utilized.

City staff is still analyzing the intersection of North Dawson Drive and Petit Street. Possible alternatives include a T-Intersection or a Sweeping Right Intersection, to fully serve the Flynn Road Industrial Area into the future. The extent of ROW acquisitions will vary, depending on selected alternative. While the Sweeping Right Intersection (Figure 5-3) is the preferred configuration at this time, the ultimate design will be based on future development and traffic demands in the area.

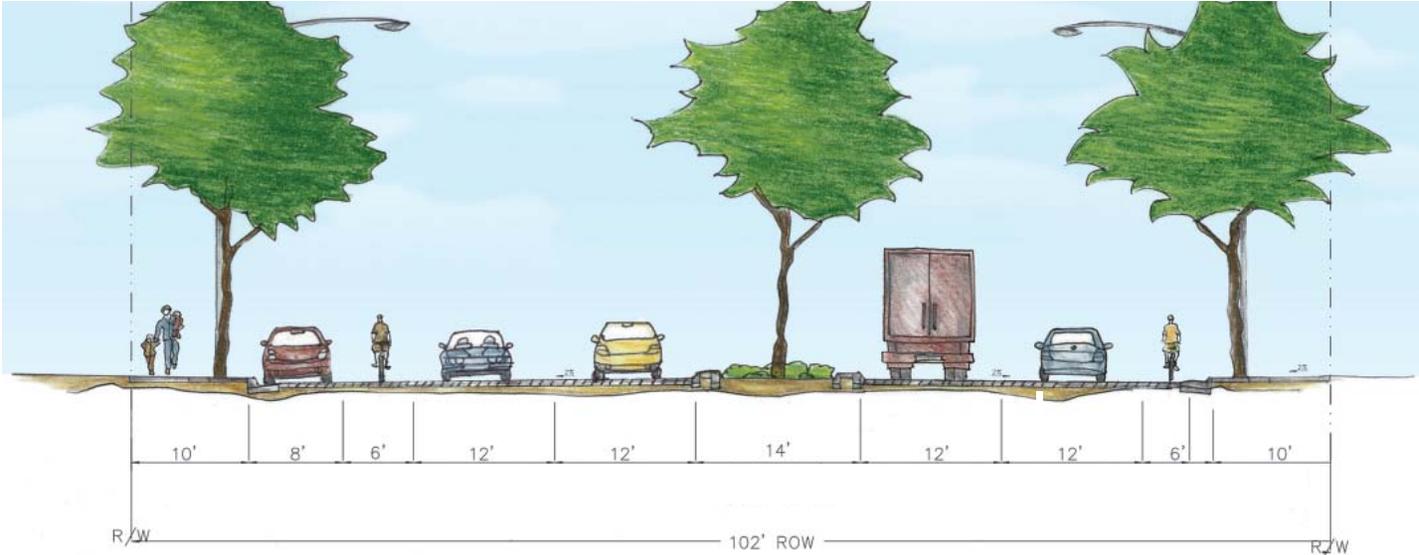


FIGURE 5-1
Petit Street Section, Looking West



FIGURE 5-2
North Dawson and Petit
"T" Intersection Alternative
Source: City of Camarillo



FIGURE 5-3
North Dawson and Petit
Sweeping Right Intersection Alternative
Source: City of Camarillo

DAWSON DRIVE

Dawson Drive is arguably the most important street in the planning area. It is the conduit that serves a majority of the businesses in the area. It also links Pleasant Valley Road and the development north of Highway 101 and provides a connection, though circuitously, between the Village at the Park and Old Town Camarillo. As proposed, the northern portion of Dawson Drive is designed to accommodate new pedestrian oriented infill development that will support the specialty retail uses as envisioned for the area. Lower Dawson is seen as much more utilitarian to accommodate existing uses and future uses that are compatible with the existing.

The suggested Section for northern portion of Dawson (Lewis overpass to 101 overpass) includes a variable 10'-15' wide walkway / landscape area (with street trees/ planting parkway), 5' bike lanes, 7' parking lane (east side only – the parking needed for any development on the west side of Dawson can be accommodated on site) 11.5' travel lanes with and a 11.5' median / turn lane. Median landscaping can be provided where it does not conflict with turning movements.

The suggested Section for lower Dawson Drive (Lewis overpass south to Pleasant Valley Road) includes a 5' wide walkway area, 8' parking lane, 5' bike lanes, 12' travel lanes. In order to provide median / turn lane some combination of reduction to bike and or parking facilities would be required.

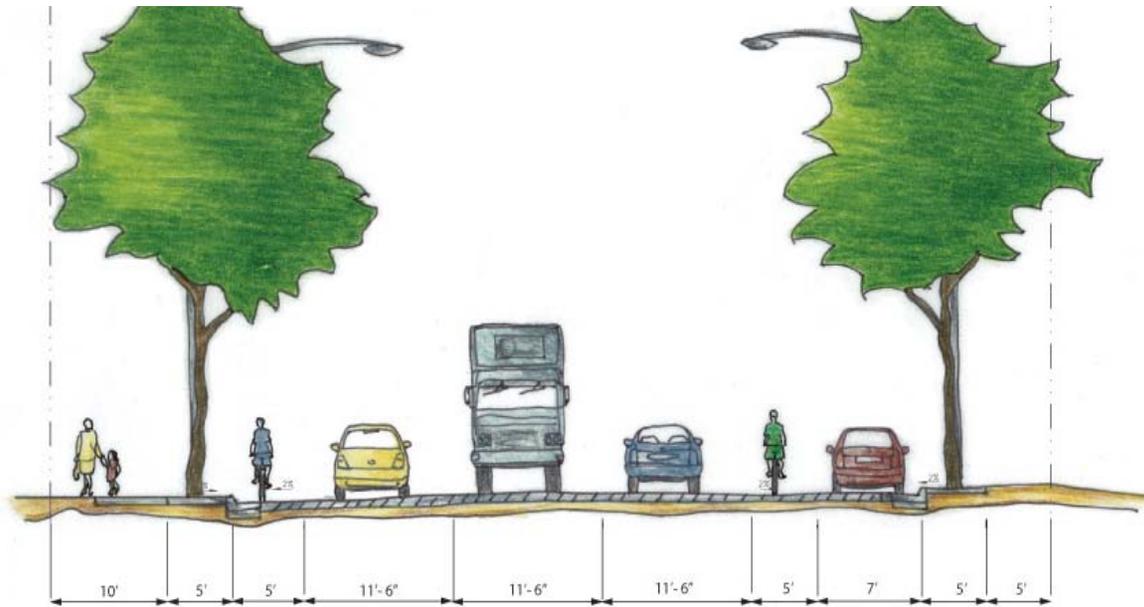


FIGURE 5-4
Northern Portion Dawson Drive Section, Looking North

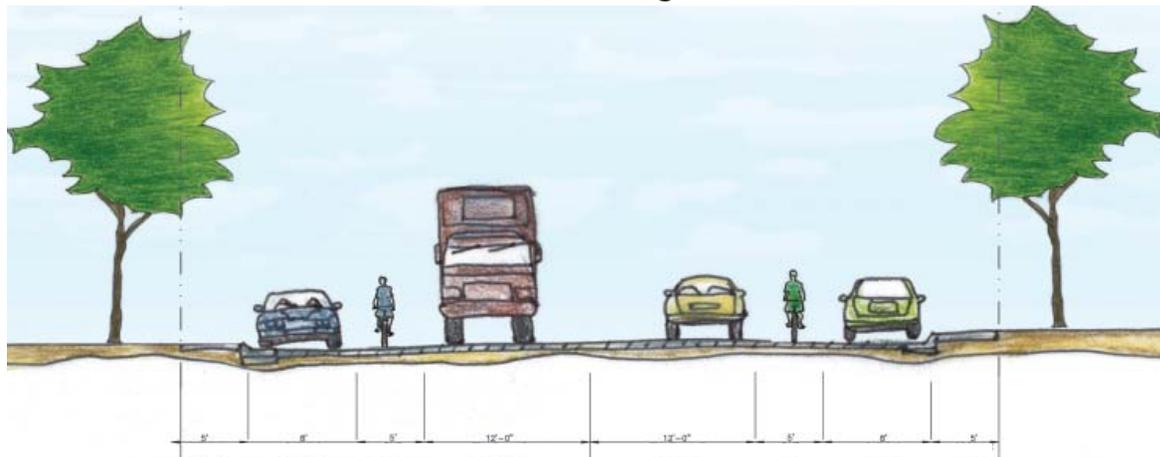


FIGURE 5-5
Lower Dawson Drive Section



Example of a meandering path integrated with landscaped stormwater detention.

LEWIS ROAD

Key features proposed beyond the Caltrans improvements include a screen wall and landscaping adjacent to the south Dawson Drive businesses on the west side of Lewis and a 10' meandering Class 1 bike path and landscaped meandering storm water detention basin on the east (south of the Lewis crossing). As Lewis begins its ascent over the tracks the storm water detention basin and screen wall terminate and the bike path continues north to connect with the on-street bike lane on the upper section of Dawson

Also see the discussion on Gateways and Wayfinding, which follows.

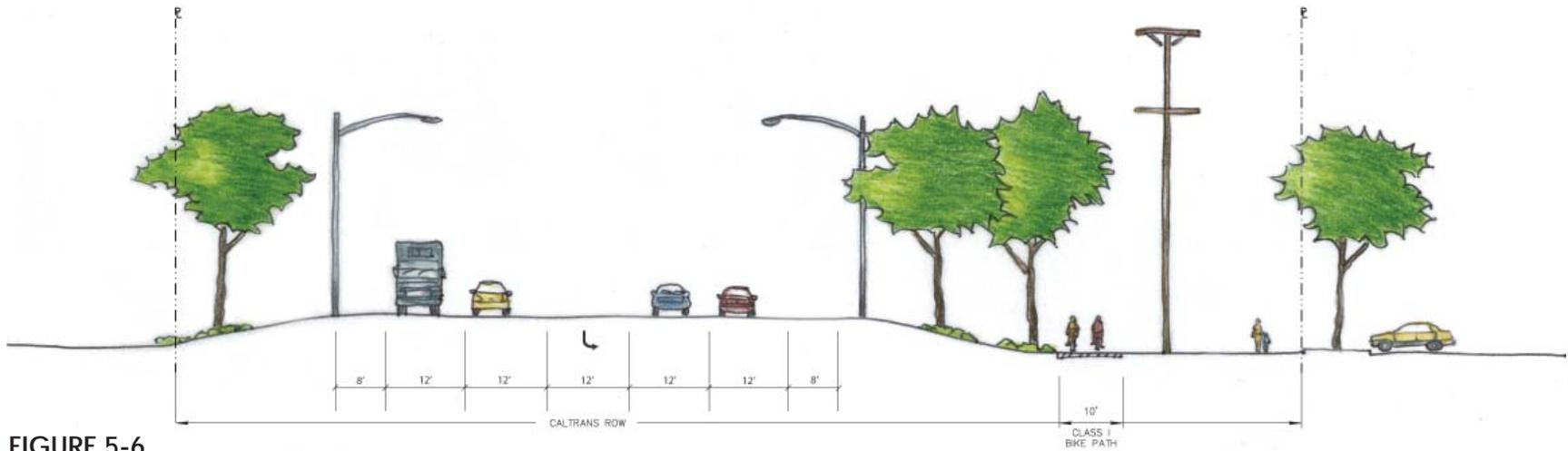


FIGURE 5-6
Lewis Road Section at New Bridge Approach, Looking North

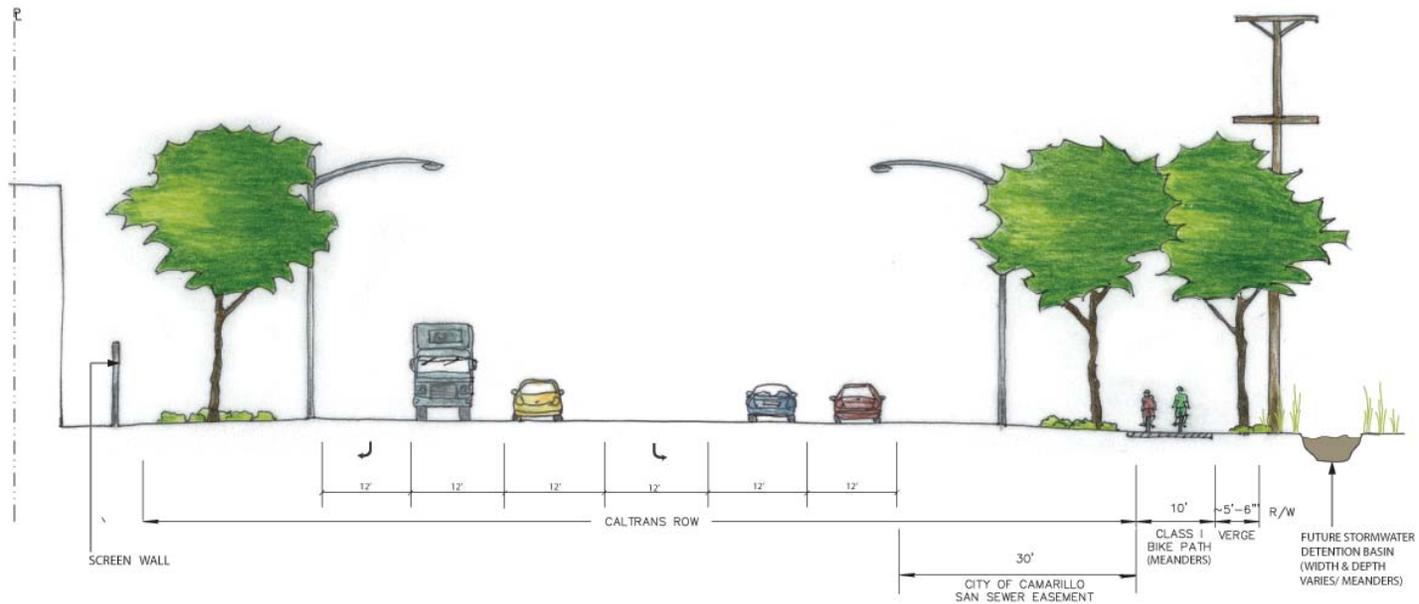


FIGURE 5-7
Typical Lewis Road Section, Adjacent to Fields, Looking North



This grade separated crossing is serviced by elevator and an enclosed crossing span. The capital and maintenance cost for this type of crossing are considerable when compared to the existing facility in Camarillo.

RAILROAD CROSSING

The community has expressed a desire to provide a better connection between the Dawson Drive area and the Metrolink Station in Old Town. The existing elevated crossing is in an excellent location to service Old Town, the North Dawson Drive sub-area, and the potential Transit Oriented Development. However, the elevated crossing is long, noisy, and unsheltered; as a result it is not used to its fullest potential. There are two alternative solutions to this problem: 1) retrofit the existing bridge with an elevator and enclosed crossing span, or 2) provide a direct at-grade crossing.



Existing pedestrian/bicycle crossing is rarely used because of the required length of travel, change in grade, and the unpleasant environment (weather, noise, and air quality) due to its proximity to Highway 101.

Retrofitting the existing bridge would be expensive and require ongoing preventive maintenance. Currently, a stairway exists at the west side of the tracks, but only a ramp on the east side. As a result, the perceived and actual distance of travel is great. A secondary stairway would improve access, but would still be a relatively slow means of crossing the tracks. The existing pedestrian overpass could also be made to be more attractive and appealing to users through the use of decorative elements or by enclosing the overpass entirely.

According to the California Public Utilities Commission (CPUC), the appropriate traffic control system to be used at a pedestrian-rail at-grade crossing should be determined by an engineering study performed by a diagnostic team. The diagnostic team must, at a minimum, include CPUC staff and parties responsible for the pathway and railway. A diagnostic team should evaluate and determine the appropriate design based on pedestrian traffic, pedestrian accident history, train operations, traffic signal preemption requirements, sight distance and site specifics relating to geometry, among other factors. Final crossing design is subject to approval by CPUC. Additional information from the CPUC can be found at: <http://docs.cpuc.ca.gov/PUBLISHED/GRAPHICS/83568.PDF>



At-grade crossings are the most cost effective in terms of capital and maintenance costs and they are the most expedient means of crossing the tracks.

CALLEGUAS GARDENS TRAFFIC CALMING



Bird's eye view of Calleguas Gardens Neighborhood.

The existing Calleguas Gardens neighborhood sits immediately adjacent to the Redevelopment Area and is the residential community most directly affected by any future changes in the area. The neighborhood is bounded by Petit Street to the north, the Residence Inn site to the east, the former 3M / Imation site to the south, and Dawson Drive to the west. A primary concern of the residents is that vehicular circulation be discouraged from using the neighborhood as a short-cut to other routes. Since these are public roads, restricted access to the neighborhood is not feasible. However, traffic calming measures can mitigate the traffic impacts by discouraging through traffic and by reducing speeds of traffic that do use the local residential streets. The map below indicates the recommended traffic calming measures. Bulb-outs would be utilized at the two main entries to the neighborhood. These will create a physical gateway and the reduced street width will choke traffic and reduce speeds. Within the neighborhood, four traffic humps have been located to discourage speeding.



Typical "bulb-out"



Typical "traffic hump"



FIGURE 5-8
Recommended Traffic Calming for Calleguas Gardens



Existing lack of sidewalk continuity on South Dawson Drive.



Existing sidewalk obstacles on North Dawson Drive.

PEDESTRIANS AND BICYCLES

The existing conditions for pedestrians and cyclists within the study do not promote a walkable community. Provisions of walkways and bicycle lanes are inconsistent, and there are many barriers that interfere with accessibility, especially for handicapped users.

The following two maps show the existing pedestrian network and the recommended improvements to that network. In addition to providing continuous and accessible sidewalks where deficiencies exist, the plan recommends the addition of a pedestrian at-grade crossing at the Metrolink station, as well as a new pedestrian and bicycle parkway and a future connection to the Village at the Park neighborhood to the east. The parkway and future connection are described in the following section.



Bicycle lanes are poorly marked or nonexistent.

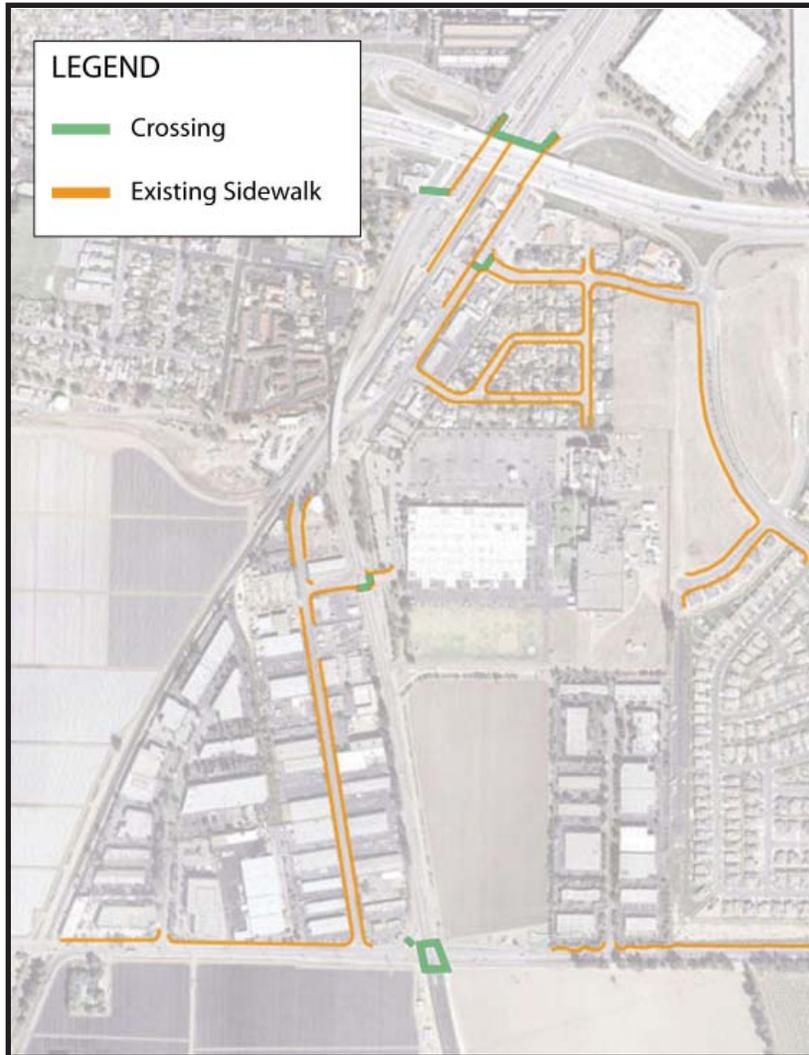


FIGURE 5-9
Existing Pedestrian Circulation



FIGURE 5-10
Recommended Improvements to Pedestrian Circulation



FIGURE 5-11
Existing and Proposed Bike Routes



FIGURE 5-12
Recommended Parkways

PARKWAYS

A new pedestrian and bicycle parkway is proposed along the eastern side of Lewis Road, extending from Pleasant Valley Road up to North Dawson Drive. This parkway provides a direct link between North Dawson Drive and points to the south, including the development opportunity sites of the 3M / Imation, the EJM Agricultural parcel, and to a bicycle route that will lead to the CSUCI campus. The parkway is separated from vehicular traffic; it will be planted with trees, shrubs, and ground cover to create a visually appealing and shaded path that is functional and attractive as a scenic route into Camarillo. As redevelopment projects are proposed in the future, the plan also anticipates a pedestrian and bicycle-friendly new connection to the existing cul-de-sac at Westpark Court, part of the Village at the Park neighborhood.



Recommended New Class One Pedestrian/Bicycle Parkway along Lewis Road Separated by Landscaping



SECTION 6

PLACEMAKING

GATEWAYS AND WAYFINDING

Cities have successfully used gateways and wayfinding systems in order to help create a sense of place and to direct people between various destinations. Ideally, the scale and character of a neighborhood's buildings and streetscape should signal one's arrival or departure from one neighborhood to the next. Gateways and wayfinding offer ways to accentuate these borders and to ease travel, which reinforces one's awareness of location and helps to create a "sense of place."

Gateways should be located at critical nodes where travelers enter the neighborhood, either by foot, by bicycle, by car, or by transit. They should be designed to be compatible with the district's architectural character, and should be scaled appropriately to reflect the importance of the district within the overall city. Gateways can incorporate multiple elements, including over-street archways, enhanced street paving, monument signs, enhanced landscaping, water features, architectural elements (such as gazebos, benches, etc.), and installations of public art.



FIGURE 6-1
Recommended Primary and Secondary Gateway Locations

Lewis Road at Pleasant Valley Road

This gateway (see Figure 6-2) is primarily a vehicular entry and secondarily a potential bicycle entry into both the Dawson Drive Industrial Area and the City of Camarillo as a whole. Therefore, it is essential that this gateway functions at the scale of the automobile, but that it should also incorporate directional signage at the scale of the pedestrian or cyclist using the landscaped pathways. As this intersection is at a prime threshold between the City and the C.S.U. Channel Islands campus to the south, the gateway should primarily announce entry into the City. The signage should be consistent with the theme and logos established elsewhere throughout the district and the city.

The recent reconfiguration and widening of Lewis Road presents two opportunities for the placement of the gateway. On the northwest corner of the new intersection, a new landscaped area exists that could accommodate a large monument sign and enhanced landscaping. Additionally, on the northeast corner of the new intersection, the start of a new landscaped pedestrian and bicycle pathway separated from the roadway presents an opportunity to install a pedestrian-scaled gateway and signage element for pedestrians and cyclists.

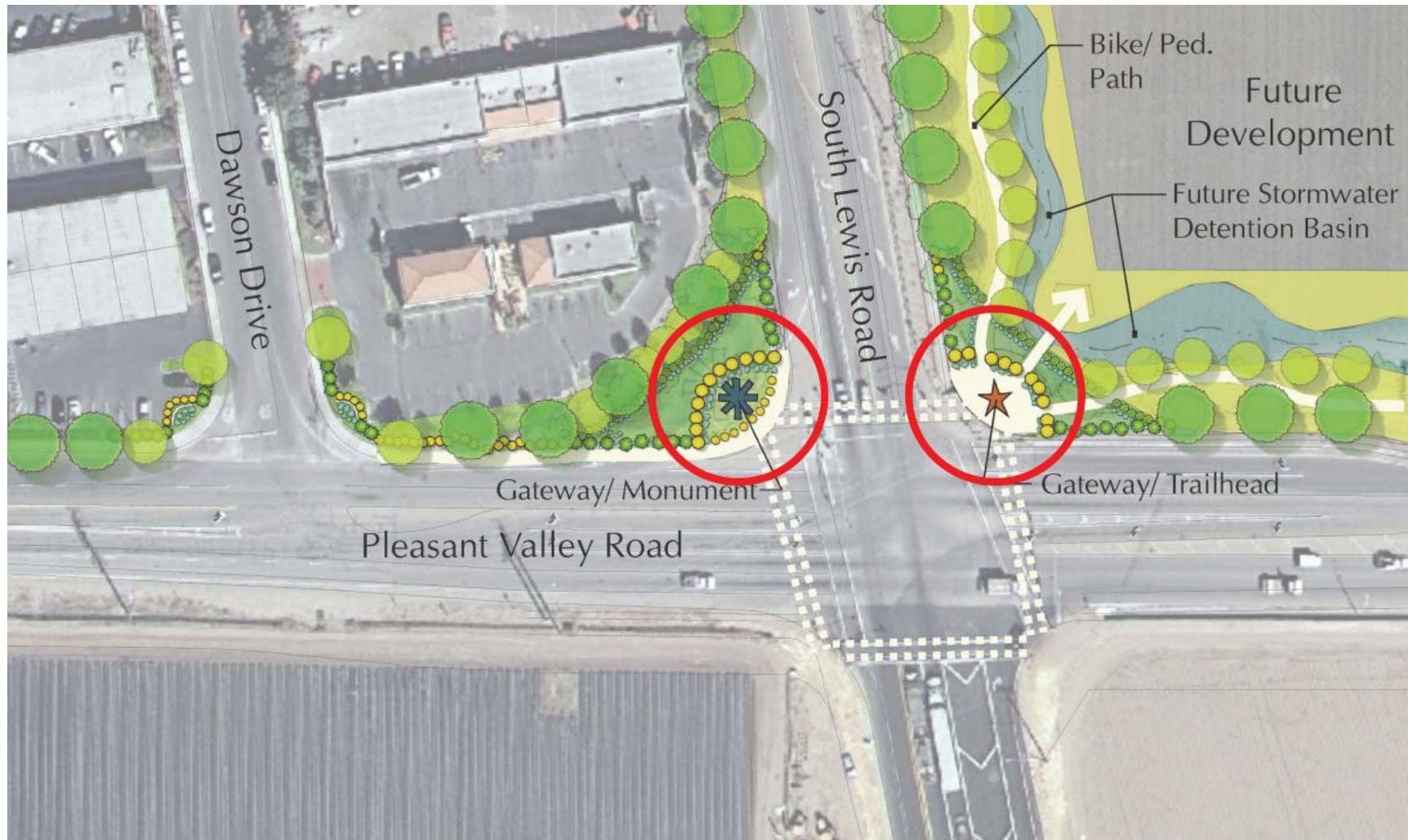


FIGURE 6-2
Potential Gateway Improvements at Lewis Road and Pleasant Valley Road

Dawson Place

The intersections at Dawson Place / Dawson Drive and Dawson Place / Lewis Road serve as critical vehicular thresholds between the Imation site, Dawson Drive, and the historical downtown (see Figure 6-3). The primary function of this gateway, therefore, is to orient and direct drivers, cyclists, and pedestrians to their destinations. A secondary function of the gateway is to provide a visually appealing architectural, landscape, or artistic element to the otherwise utilitarian landscape.

This intersection of Dawson Place and Dawson Drive is primarily used for travel between the “Lower” and “Upper” Dawson Drive areas, and for those moving from Dawson Drive to Lewis Road, and vice versa. Any gateway or wayfinding elements, therefore, need only to serve a functional directional purpose. The signage should be consistent with the theme and logos established elsewhere throughout the district and the city.

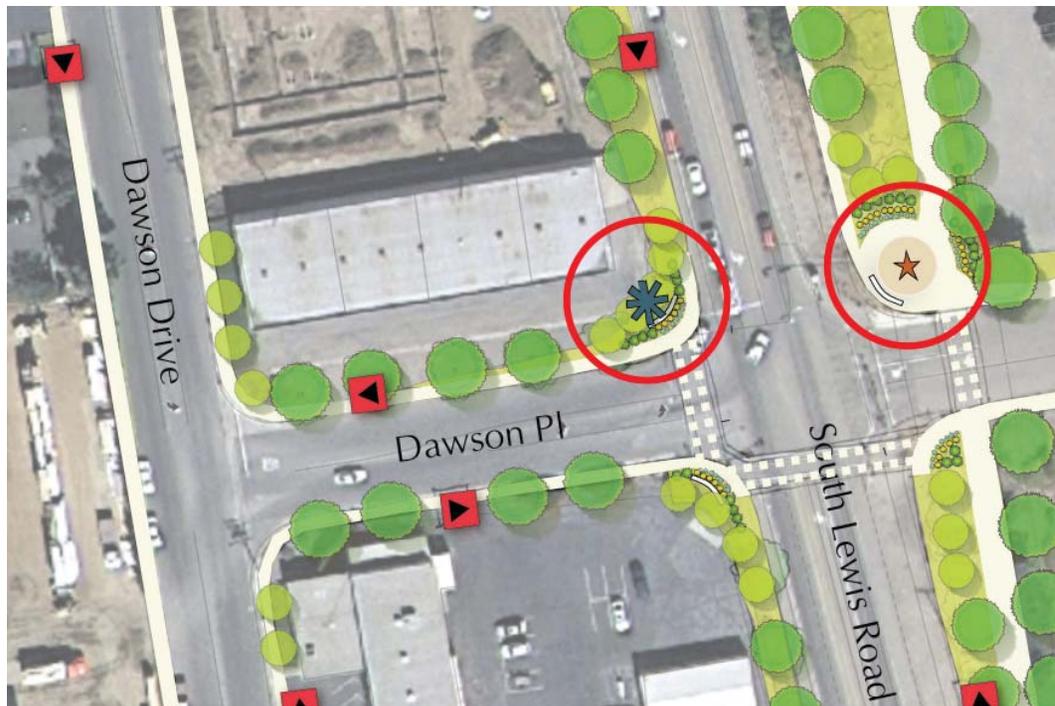


FIGURE 6-3
Potential Gateway at Lewis Road and Dawson Place / 3m Drive

Dawson Drive at Pleasant Valley Road

This intersection (see Figure 6-4) is primarily used for access to the “Lower” Dawson Drive industrial area from the south. Because of the industrial nature of the street and the business-serving traffic along the Lower Dawson Drive area, it is not desirable to direct additional through traffic to this road. Therefore, the gateway at this intersection should only serve to announce entry into the industrial area.



FIGURE 6-4
Potential Gateway at Pleasant Valley Road and Dawson Drive

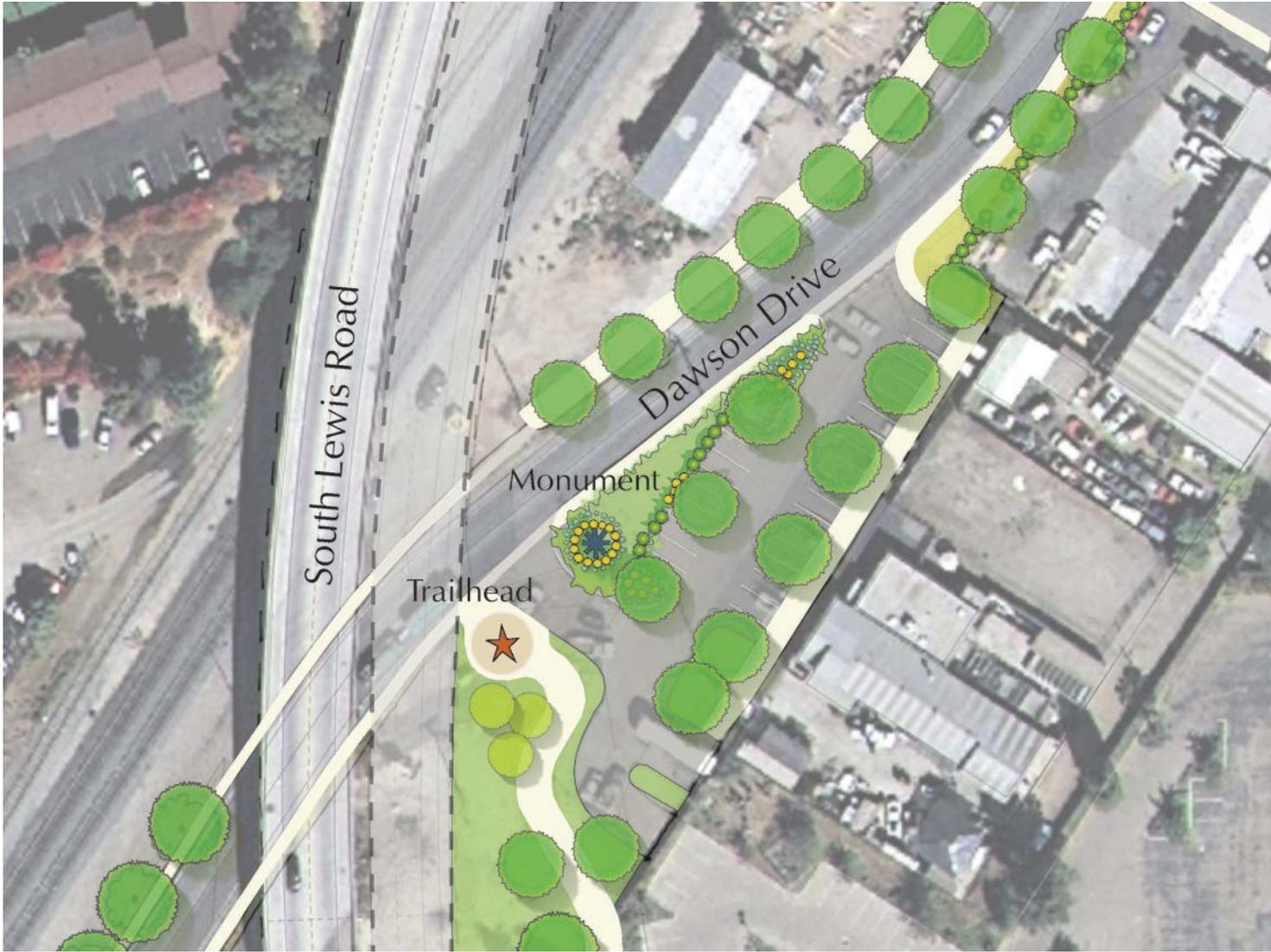


FIGURE 6-5
Potential Improvements to Create the Triangle Gateway

Dawson Drive at Triangle (north of Lewis Bridge)

This proposed gateway (see Figure 6-5) is located at the southern entry into the “Upper” Dawson Drive Industrial Area. At this point, the character of the area changes from a more utilitarian and contemporary industrial park into a more mixed, historically significant area. The Upper Dawson Drive area is characterized by a historical swath of older, deteriorating, rail-serving industrial buildings, several eclectic light-industrial uses, and an adjacent residential neighborhood. The “triangle” site is currently a remainder right-of-way under the control of the City of Camarillo. Several local businesses have addresses adjacent to this triangle, but the triangle itself is currently informally used for parking and for storage of trucks. These uses are inappropriate for the area and contribute to the blighted character of the neighborhood. Further, the triangle lies at the end of the view as one travels south on Dawson Drive, so the site creates the visual backdrop for the street.

This document recommends a significant reconfiguration of the triangle, to formalize parking that serves the local businesses, while improving the aesthetics of the area by introducing landscaping, hardscape and other gateway elements. The primary function of this gateway area is to establish a new character for the area, building upon the “Historic Railroad” architectural character of the existing buildings. The character of this area is more pedestrian-scaled and eclectic, and these should be reflected in the design of the gateway. Public art installations would be ideal for this gateway



FIGURE 6-6
Potential Gateway Improvements at Dawson Drive and Petit Street

Dawson Drive at Petit Street

This intersection (see Figure 6-6) lies at the terminus of Petit Street traveling west. Formerly, this street would have continued as the historic Ventura Boulevard, but it has long since lost that connection. Instead, the intersection directs traffic either to the Camarillo Ranch House and Flynn Road Industrial Area north of the 101 Freeway or south to the Dawson Drive Industrial Area, historic Ventura Boulevard or the CSU Channel Islands Campus. These multiple destinations are not mentioned on existing signage, with the exception of the Camarillo Ranch. In addition, this intersection represents a node for those wishing to cross the railroad tracks to access the Metrolink Station or the historical downtown. Currently pedestrians and cyclists must travel north on Dawson Drive to access the overhead walkway north of the 101 freeway. In the future, this document envisions an additional at-grade pedestrian crossing over the rail tracks to ease movement between both sides.

This gateway must function on several levels. First, it must provide directional assistance to drivers so that they can reach their destinations without confusion. Second, it must provide wayfinding to pedestrians and cyclists to improve access to the transit station. Third, it should also provide an appealing visual terminus to Petit Street. This location is of additional importance because of the views across the tracks into the historical downtown and of Saint Magdalene Church. Finally, this location could also serve as a public open space that would serve a rehabilitated mixed-use zone located between the rail tracks and Dawson Drive. Such an open space could function as a gathering space or accommodate outdoor cafes.



FIGURE 6-7
Potential Gateway Improvements at Petit Street and Village at the Park Drive

Petit Street at Village at the Park Drive

This gateway (see Figure 6-7) lies immediately south of the exit ramp of the 101 freeway, at the juncture between the freeway, the Village at the Park development, and the Calleguas Gardens / Dawson Drive area. Travelers entering the Dawson Drive area may be traveling either into the area itself, toward the potential future transit-oriented development, or to further destinations such as Camarillo Ranch House, Flynn Road Industrial Area, or the historic downtown.

This gateway must provide directional assistance to drivers. It should also provide a visually attractive entry into the area. Because this intersection is not yet in the Dawson Drive area, this gateway should not announce entry into the Dawson Drive Industrial Area.



FIGURE 6-8
Proposed Screening Location Map

SCREENING

In order to improve the general image of the study area, visual screening is recommended for three specific segments along the railroad corridor and along Lewis Road. Figure 6-8 indicates the locations and extent of the recommended screening.

Screening Segment 1

Screening along the west of Lewis Road is highest priority. The recent removal of the trees along the roadway, which accompanied the road widening project, has revealed an unattractive view from the highway of the back side of the industrial buildings along lower Dawson Drive. In order to create an attractive and homogenous appearance, a new, minimum 10 feet tall, decorative screening wall is recommended for this location. This wall shall include the use of anti-graffiti coating to allow ease of removal. In addition, additional landscape, beyond that which will accompany Caltrans' road widening project, is required to create a more pleasant view from the highway.

Screening Segment 2

Screening of the railroad-adjacent properties on the west side of Calle San Pablo is a secondary screening priority. In this area, there is less need to screen storage areas from the railroad tracks. Instead, the buildings themselves are a target of graffiti. In this case, landscape screening is recommended in order to make the buildings less appealing as targets.

Screening Segment 3

A third screening segment will be added adjacent to the railroad tracks at the North Dawson area from the 101 overcrossing to the Lewis Road Bridge in order to retain the view corridor through the area through the use of wrought iron.



Examples of Appropriate Decorative Screen Walls



Drainage and water detention integrated with landscape.



Example of front yard landscaping.

LANDSCAPE (ON-SITE)

The City encourages landscaping improvements to existing properties within the study area. Desirable improvements include the planting of shade trees within existing paved or parking areas; the planting of street trees adjacent to public rights-of-way; and the retrofit of existing sites to provide additional onsite storm water retention.



Example of a shaded parking lot.

STREET FURNISHINGS

This concept plan aims to improve the function and the beauty of the area. Emphasis is placed on creating an inviting space at the pedestrian scale.

- At future major transit stops provide uniquely designed, covered bus shelters that include benches, route maps, schedules, lighting, trash, and recycling receptacles.
- At all future bus stops, provide a bench, route maps, schedules, and pedestrian-scaled lighting.
- Consider using a color such as green, brown, or bronze rather than black to paint street light standards, to provide a distinct character.
- Install distinctive street name signs as was done in Old Town.



For ease of maintenance, street furniture should be consistent with Old Town.



Example of a covered bus shelter.



Example of distinctive street sign & painted streetlight.

NEIGHBORHOOD SIGNAGE

The City of Camarillo is comprised of many diverse neighborhoods, some as old as the City itself. The Calleguas Gardens neighborhood is a special place for its residents, a place to be treasured and nurtured. The ability of each resident to identify with their neighborhood contributes to a sense of community pride.

To ensure that this neighborhood is recognized, the City should develop the Neighborhood Identification Sign program for the fabrication and installation of new neighborhood identification signs. To guarantee a uniform appearance throughout the City, a single neighborhood identification sign design should be considered, however, individual neighborhood culture or history should be highlighted in the design of that neighborhood's sign.



Examples of neighborhood identification signs.



Example of neighborhood identification sign.

SECTION 7

INFRASTRUCTURE

Utility systems within the Dawson Drive Redevelopment Area (study area) are generally adequate to service existing buildings and infrastructure, and can be adapted using prevailing techniques and practices to accommodate reasonable anticipated growth. While current domestic water supply, storage and main line distribution systems are generally considered adequate, planning should consider both conservation practices to offset drought conditions and incorporation of future uses for recycled water when available. Sanitary sewer trunk lines, lift stations, force mains, and treatment plant capacity are considered adequate, and future development should be serviced by normal extensions of the sewer collection system although some affected existing lines may require evaluation of structural integrity for continued use. While natural gas supply and local distribution facilities are considered adequate, new projects should be encouraged to incorporate the latest advances in energy efficient appliances and conservation measures. Both the overhead and underground component portions of the electrical power backbone network are considered adequate, and future planning should include provisions for energy efficiency, conservation, renewable energy, and opportunities to remove overhead facilities.

Storm drains are similarly viewed as generally adequate to serve the area with the exception of a few noted trouble spots along Dawson Drive itself. Where not already resolved by recent construction, the trouble spots can be resolved in the normal course of redevelopment or by specific private or public capital improvements. New project proposal proponents should make early contact with appropriate City staff. Through early contact applicants will better be able to verify requirements for storm drain lines and other drainage facilities. Applicants should also be able to determine if existing drains will need modification as part of a proposed project development plan. The City staff will also be able to provide information about the latest storm water quality ordinances and regulations. Expected storm water quality requirements in the near future include complete capture and infiltration or reuse of the volume of runoff produced from a 0.75 inch storm, reduction of effective impervious area for a project to 5 percent or less for new development and 30 percent or less for redevelopment, and incorporation of other smart growth techniques to reduce the impacts of developments on local and regional watersheds. New orders regulating storm water discharges both during and after construction were promulgated in 2009 with scheduled implementation actions into subsequent periods for municipal separate storm sewer systems (MS4).

WATER

Water is distributed throughout the Dawson Drive Redevelopment Area (study area) as described in the Water System Master Plan Update, dated December 1999, prepared by Parsons Engineering Science. There have been a few changes in the water system infrastructure as shown on the Water Infrastructure Map. In general, there are now more points of connection with the City’s water system infrastructure via 12-inch diameter waterlines. In addition, a portion of the 16-inch waterline in Lewis Road has been downsized to an 8-inch diameter for approximately 1200 feet. During the construction of the infrastructure to the East (Village at the Park Development), additional waterlines were constructed to provide improved fire flows. There is now adequate water pipeline infrastructure to allow for fire protection and water service with the added looping for the onsite development, although hydraulic capacity will need to be verified for any significant development. It is also recommended that the City or future development proponents replace the downsized 16-inch diameter waterline in Lewis Road, should fire flow requirements be increased for future developments.

Regarding the sources of supply and storage, the City has indicated that both of these issues have been generally satisfied by their existing facilities except for the overall reduction in supply caused by the current drought and other import restrictions at the statewide level.

The City does not have a formally developed plan at the present time to extend recycled water into the Dawson Drive area. Depending on the scope of proposed new projects it is possible that the City may require applicants to consider the extension of infrastructure to deliver available recycled water for appropriate uses.

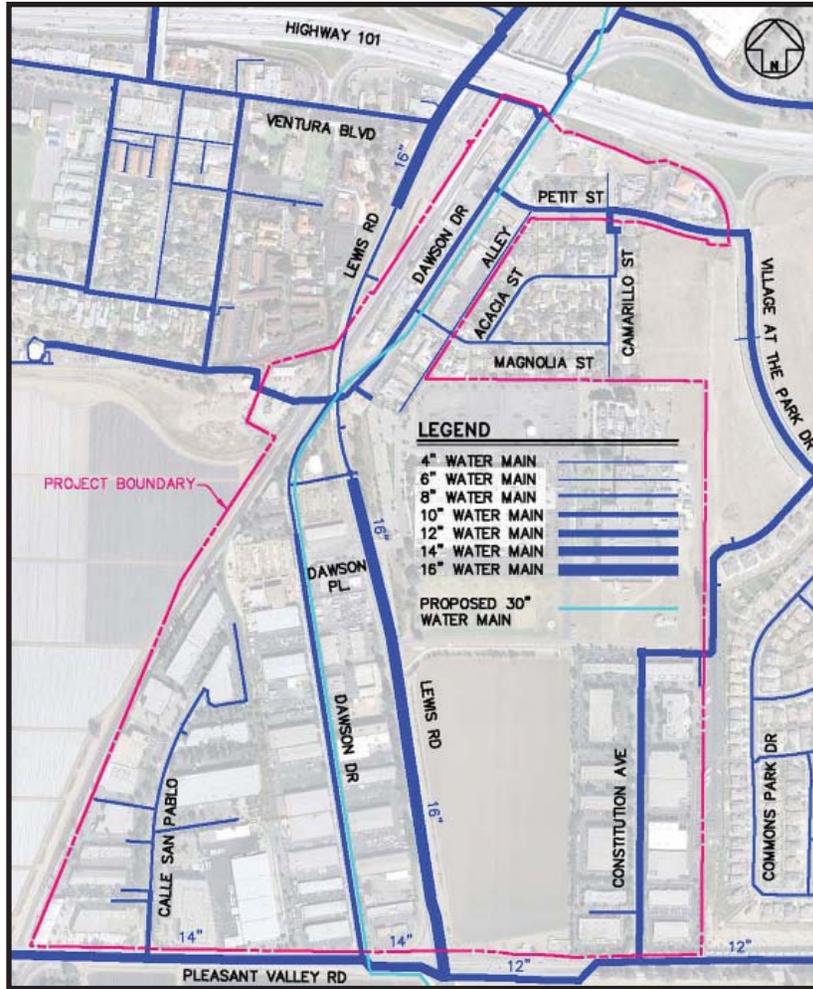


FIGURE 7-1
Existing and Proposed Water Mains

SANITARY SEWER

The sanitary sewer system which serves the Dawson Drive Redevelopment Area (study area) is described in the Camarillo Sanitary District’s Wastewater System Master Plan Update, dated December 1999, prepared by Parsons Engineering Science. A sewer collection system evaluation (Sewer Evaluation Capacity Assessment Plan - SECAP) updated the 1999 master plan that the Camarillo Sanitary District approved in July 2009, identified that the local sewer distribution network in the proposed project area is at design capacity. Hydraulic capacity and structural integrity of sewer facilities (main sewers, pump stations, main trunk sewers and wastewater treatment plan) will need to be verified for any significant development. The Sewer Infrastructure Map shows the existing sanitary sewer facilities within the study area. In general, the sewers flow from North to South, and eventually are conveyed into the Camarillo Sanitary District’s trunk sewer, which flows in a west to east direction along Pleasant Valley Road. The undeveloped areas can be served by constructing in-fill collection pipelines which can drain into the Lewis Road trunk sewer, or into an extension of the 8-inch diameter trunk sewer which is now within Constitution Avenue. This extension could be extended easterly to the easterly edge of the study area to serve the properties further to the north. In summary, the Sanitary Sewer infrastructure needs can be met by the in-fill facilities needed for new development.

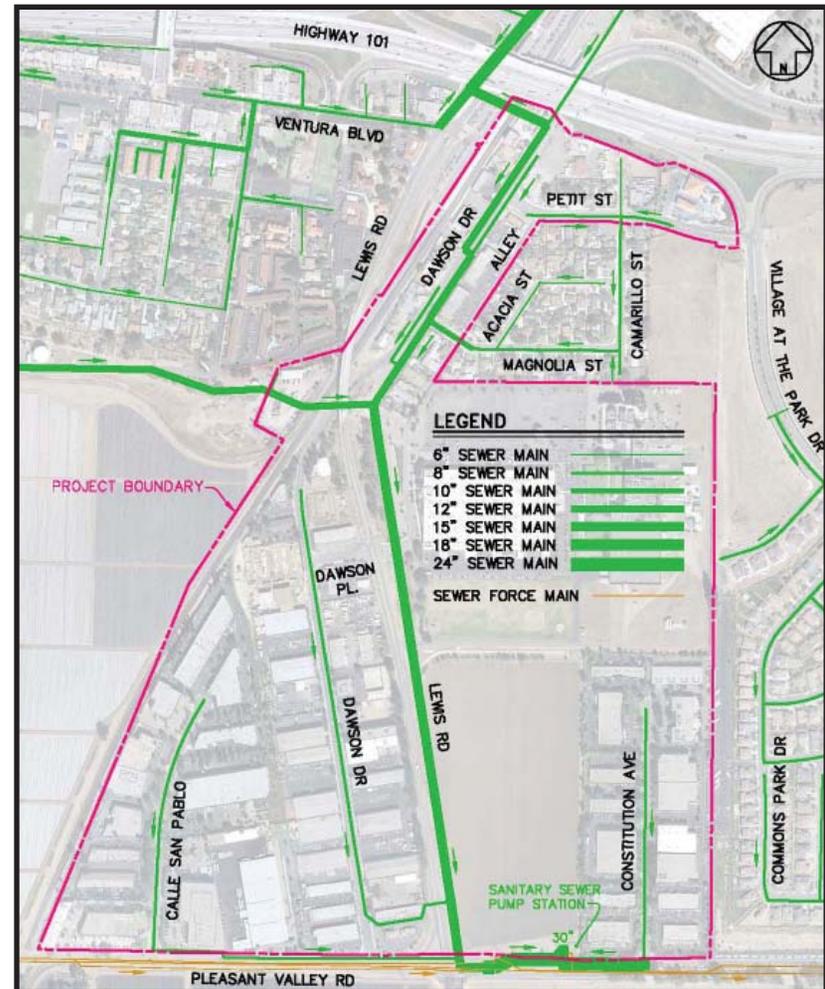


FIGURE 7-2
Sanitary Sewer Mains

NATURAL GAS

Natural Gas for the Dawson Drive Redevelopment Area (study area) is provided by the Southern California Gas Company. The gas lines are shown on the Gas Infrastructure Map. Natural gas backbone lines are considered adequate for existing needs and can be expanded as needed for reasonably foreseeable needs under the City's current general plan. New project applicants are encouraged to contact the Southern California Gas Company as early as possible during planning in order to assure adequate provisions for service can be made in a timely manner. New projects should include provisions for energy efficient appliances and other conservation measures.

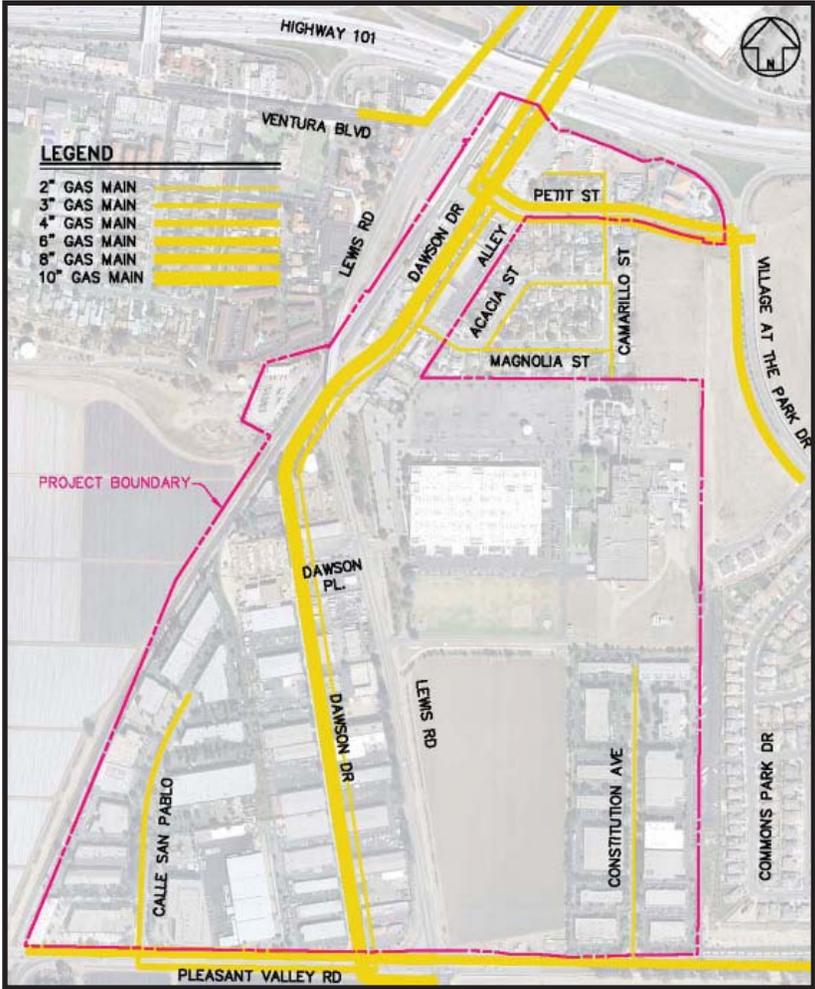


FIGURE 7-3
Natural Gas Mains

STORM DRAINAGE AND NPDES PERMIT

Storm Drains within the Dawson Drive Redevelopment Area (study area) are shown on the Storm Drain Infrastructure Map. The drainage system is described in the City of Camarillo Master Plan of Drainage dated April, 1998 prepared by the consulting firm Hawks & Associates (Hawks). Drain lines of larger size and capacity include a portion of the Lewis Road Drain under the jurisdiction of the Ventura County Watershed Protection District (VCWPD). This VCWPD drain is administered separately from the City system, and therefore is mentioned but not detailed in the 1998 Master Plan of Drainage.

A reach of the VCWPD Lewis Road Drain from Pleasant Valley Road to approximately 1,500 feet upstream has recently been constructed as a reinforced concrete box (RCB). This is being done in connection with the Lewis Road widening project that is being performed by the County of Ventura as lead agency. This new buried RCB drain may open opportunities for alternative uses over the surface such as a bikeway.

The 1998 Master Plan of Drainage did not identify any specific deficiencies for study area drains, nor were recommendations provided for any specific new drainage infrastructure.

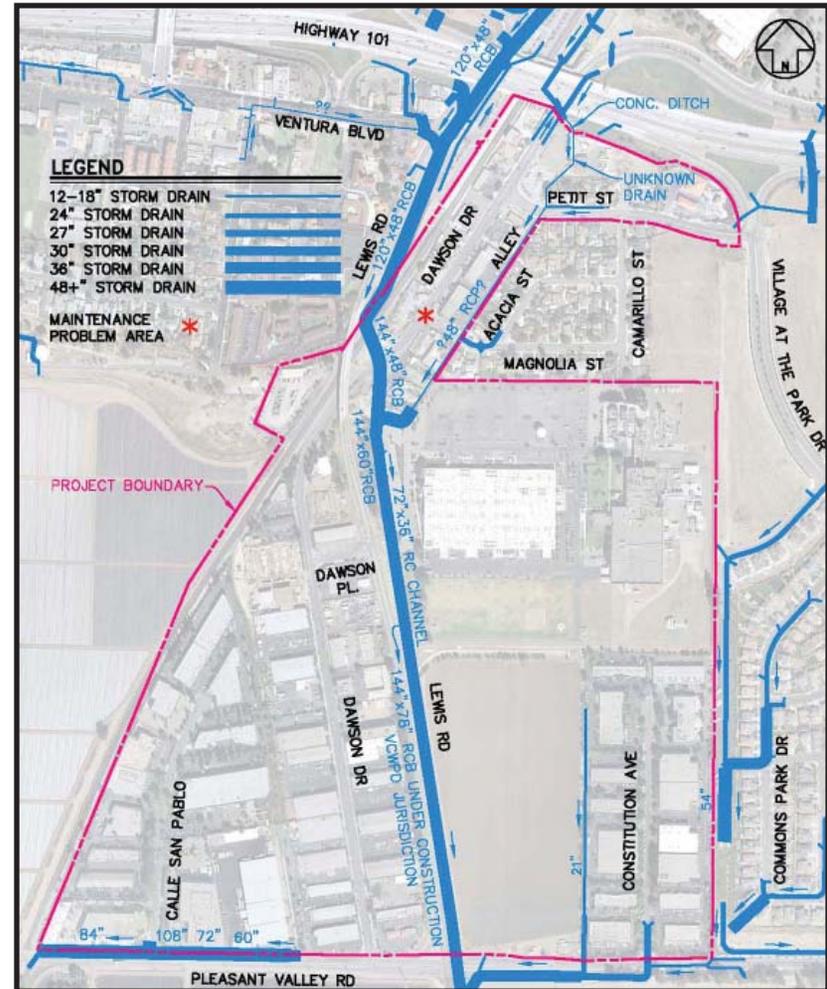


FIGURE 7-4
Storm Drainage



The update to the master plan of drainage suggests that a storm water ponding problem exists along the westerly side of Dawson Drive near the intersection of Magnolia Street.



The building finish floor is very close to top of street curb elevation.

The City is currently preparing an update to the master plan of drainage using the consulting firm Kasraie Consulting (Kasraie). Since the update is preliminary, final recommendations are not yet available. Mentioned in the preliminary update to the master plan of drainage is the City of Camarillo requirement that all storm drain lines be sized to handle runoff from a 50-year event, that is, a runoff event that has a 2% chance of occurrence in any given year. The preliminary update to the master plan of drainage indicates that existing drains may fall below the 50-year design standard in two adjacent locations within the study area. These existing drains are a 27-inch line in Pleasant Valley Road from 600 foot easterly of Lewis Road to Constitution Avenue, and the continuation of the same 27-inch line in Constitution Avenue from Pleasant Valley Road to 200 feet northerly.

The update to the master plan of drainage suggests that a storm water ponding problem exists along the westerly side of Dawson Drive near the intersection of Magnolia Street as indicated on the Storm Drain infrastructure Map. This problem could be relieved by the installation of an inlet and connector pipe southwesterly approximately 300 feet to an existing storm drain line in Dawson Drive, or alternatively southeasterly approximately 200 feet to an existing storm drain in the alley intersection with Magnolia Street.

Another site known to City staff to have a history of flooding, and an unusual drainage system, is the rental yard at 92 N. Dawson Drive, located at the northeasterly corner of Dawson Drive and Petit Street. An open trapezoidal channel passes directly under two buildings there. According to the proprietor, flooding last occurred in 1998 while Lewis Road Drain was under construction. The trapezoidal channel is said to transition into a pipe, thought to be 48-inches in diameter, under the building that is currently the main business office. This configuration should be remedied if the property is redeveloped.

Also reported to City staff as having prior flooding is the building at 555 S. Dawson Drive, two buildings northerly of Pleasant Valley Road on the west side of the street. The proprietor reports a flooding incident in 1998 that seriously impacted operations in the main office of the business. The building finish floor is very close to top of street curb elevation. Absent any ability to raise such buildings during redevelopment, relief for flooding would require further study, design and construction of storm drains upstream on Dawson Drive as a special project.

In connection with potential redevelopment activities within the study areas, new project proposal proponents should make early contact with appropriate City staff. Through early contact applicants will better be able to verify requirements for storm drain lines and other drainage facilities. Applicants should also be able to determine if existing drains will need modification as part of a proposed project development plan. The City staff will also be able to provide information about the latest storm water quality ordinances and regulations.

Storm water quality requirements for planning and land development are contained in orders promulgated by the Los Angeles Regional Water Quality Control Board (LARWQCB). Along with all the other cities in the county, the City of Camarillo is a co-permittee with the Ventura County Watershed Protection District (principal permittee) under a countywide municipal National Pollutant Discharge Elimination System (NPDES) permit for stormwater discharges issued by order of the LARWQCB. Order 09-0057, Waste Discharge Requirement for Ventura County Municipal Separate Storm Sewer System, NPDES No. CAS004002, was adopted by the LARWQCB on May 7, 2009. This Order requires the updating of the Ventura County Technical Guidance Manual for Storm Water Quality Control Measures within 365 days. The development requirements of the Order shall begin 90 calendar days after the Executive Officer of the LARWQCB approves the changes to the Technical Guidance Manual. After that date all discretionary projects that have not been deemed complete for processing will be subject to the requirements contained in the Order.

LARWQCB Order 09-0057 promotes land development and redevelopment strategies that consider water quality and water management benefits associated with smart growth techniques. A key requirement is that all new development and redevelopment projects shall reduce the Effective Impervious Area to 5 percent or less of the total project area for new developments, and 30 percent or less of the total project area for redevelopments. Impervious areas may be rendered "ineffective" if the stormwater runoff from those areas



Another site known to city staff to have a history of flooding and an unusual drainage system is the rental yard at 92 North Dawson Drive.

is fully retained onsite for the design storm event. The design storm event is characterized as the volume of runoff produced from a 0.75 inch storm or the equivalent using alternative calculation methods. This runoff volume must be infiltrated, reused or evapotranspired onsite. Alternative compliance measures are allowed when strict compliance is demonstrated to be technically infeasible.

The exact range of technical options available to comply with LARWQCB Order 09-0057 will not be known until the update of Technical Guidance Manual is approved and published. Detention basins similar to those existing at Camarillo Ranch, Village at the Park and the Sares-Regis Basin serving the Camarillo Library could be visualized as samples. For compliance, such basins would need to be modified with partial retention for infiltration purposes. More dense urban projects with smaller footprints could be served by underground chambers or cisterns. The underground facilities could retain runoff for infiltration or potential irrigation reuse.

LARWQCB Order 09-0057 also prescribes specific minimum best management practices for new construction activities. Waste discharge requirements for discharges of storm water runoff associated with construction activity are also set by the September 2, 2009 Order 2009-0009-DWQ of the California State Water Resources Control Board. Reference is made to these orders for detailed information about construction activity requirements.

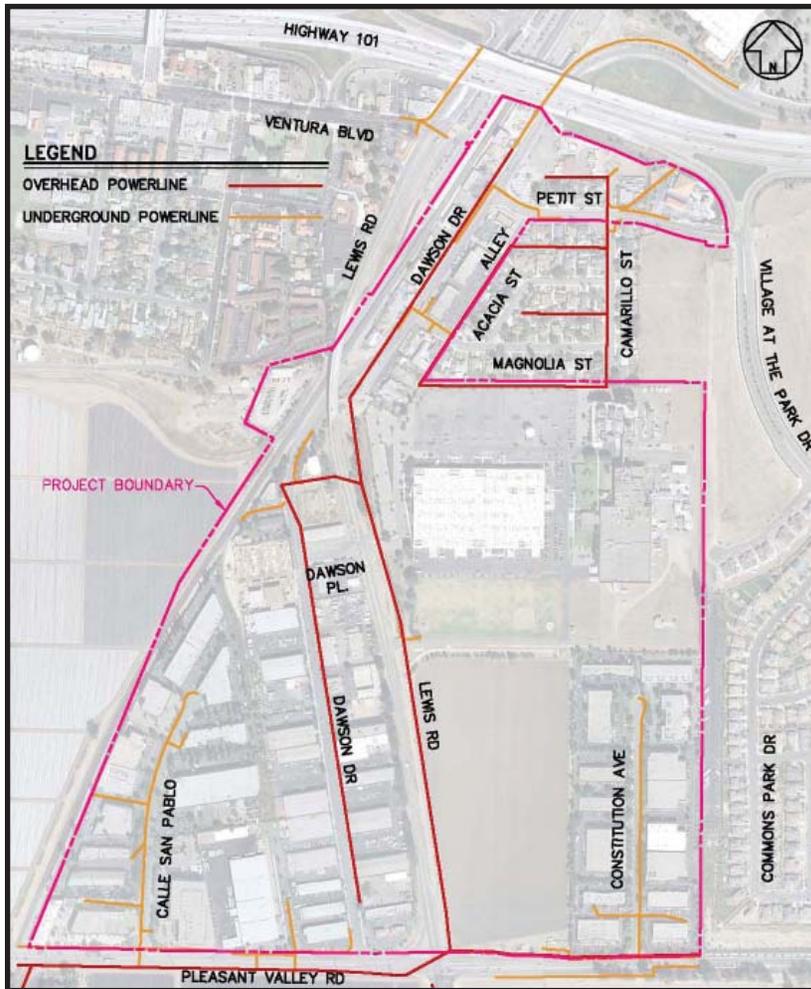


FIGURE 7-5
Electrical Power

ELECTRICAL POWER

Power for the Dawson Drive Redevelopment Area (study area) is provided by the Southern California Edison Company. Power lines in the area are both overhead and underground as shown on the Edison Infrastructure Map. The backbone power system is considered adequate for existing needs and can be expanded as needed for reasonably foreseeable needs under the City's current general plan. New project applicants are encouraged to contact Southern California Edison Company as early as possible during planning in order to assure adequate provisions for service can be made in a timely manner. New projects should include provisions for energy efficiency, conservation and potentially renewable energy opportunities when feasible.



New projects should include provisions for energy efficiency, conservation, and potentially renewable energy opportunities when feasible.

SECTION 8

ECONOMIC DEVELOPMENT and NEIGHBORHOOD ENHANCEMENT

*Economic Development Program
Neighborhood Enhancement*

In order to achieve revitalization effort goals for the Dawson Drive Industrial Area, Camarillo would also implement a coherent set of economic development and neighborhood enhancement programs designed to:

- Retain, expand and attract new businesses
- Strengthen neighborhood cohesiveness and improve overall appearance
- Encourage infill development and redevelopment of properties within the Dawson Drive area

This section describes how the City and its Redevelopment Agency would undertake a coordinated economic revitalization effort that will complement the circulation, transit, placemaking and infrastructure improvements recommended in the prior three chapters.

ECONOMIC DEVELOPMENT PROGRAM

The purpose of the economic development program is to retain, expand and attract new businesses to the area that will complement each other. A key goal of the economic development program is to enable existing property owners to improve their properties and catalyze new business growth in the area.

As recently undertaken in Old Town Camarillo, the City would work with property owners and existing businesses to improve their buildings, facades, signage and/or site access, as well as access funds to help encourage business expansion. In addition, the City would encourage new businesses and developers to invest in key business opportunities that would stimulate new economic growth. The following programs are recommended:

- Façade and Property Improvements
- Business Assistance
- Code Compliance
- Billboard Removal

Façade and Property Improvements

A key component of the revitalization effort is to make the Dawson Drive Industrial Area a more attractive place to work and do business. In order to accomplish this, property owners will be encouraged to upgrade building facades, rehabilitate buildings, enhance signage, improve building access and create more pedestrian-friendly streetscapes, with attractive landscaping and street furniture. Improved signage and access will make it easier for patrons to find area businesses by automobile or bicycle, or on foot, which will in turn encourage business growth.

Business Assistance

The City will work with local businesses and property owners to evaluate the types of business assistance programs that may be needed to improve business conditions and enhance business growth. City staff will educate the business community about the types of programs that are currently available in Camarillo, and help them access these programs. In addition, as the area improves, the City in collaboration with local businesses will undertake a marketing effort designed to raise the profile of Dawson Drive as a commercial and industrial destination.

Code Compliance and Overlay Zone

The City will identify properties that are not currently in conformance with existing City building and safety codes, as improving these properties is crucial to reducing the adverse physical conditions in the area, particularly along the railroad tracks. Proactive code compliance will help lay the groundwork for improved building conditions and private investment. The City will also determine what business practices in South Dawson Drive are not currently compliant with City codes, recommend those that are acceptable for the area, and designate those to be included in an overlay zone or a new zoning designation for this area.

Billboard Removal

The private billboard currently located along North Dawson Drive is not in compliance with the City's sign ordinance, and detracts from the area's overall appearance. The City and its redevelopment agency should further pursue its removal. The property is currently owned by Union Pacific Railroad, and the billboard is currently leased to Clear Channel.

NEIGHBORHOOD ENHANCEMENT

The purpose of the neighborhood enhancement program is to strengthen the neighborhood fabric of the Dawson Drive area, encourage infill development and reinforce the neighborhood identity of the adjacent Calleguas Gardens. The program will encourage property owners and developers to undertake strategic infill development that will complement and enhance existing businesses.

A key component of the neighborhood enhancement program is the implementation of design guidelines to assure high quality, consistently designed development that is functional, safe and attractive. (These design guidelines are presented in Section 9, which follows.) Three complementary programs are also recommended and described below:

- Neighborhood improvements
- Encourage redevelopment of the Dawson Drive area
- Infill development assistance

Neighborhood Improvements

The City will seek input from local businesses and property owners regarding the design and implementation of the proposed circulation, transit, placemaking and infrastructure improvements described in the prior three sections. To promote the walkability and attractiveness of the area, the City will collaborate with local businesses and property owners to undertake a combination of improved neighborhood identification signs, traffic calming measures, sidewalk improvements, screening and buffers.

Encourage Redevelopment of Dawson Drive Area

The City will work with existing property owners and businesses to encourage the improvement of properties and the development of transit oriented, infill development in the North Dawson Drive area, particularly in the vicinity of the Metrolink Station, where future workers and residents would benefit from easy transit access and enhanced connections with Old Town Camarillo. Given these advantages, this subarea is particularly suited to new development or reuse as live-work spaces, where artists and entrepreneurs could live in the location in which they work.

While the South Dawson Drive area will remain industrial in character, the North Dawson Drive area may be considered for rezoning to allow opportunities for live-work housing that would appeal to a variety of families and individuals who live and work in Camarillo. In the near term, the City may review the possibility of rezoning the North Dawson area to be consistent with the

Camarillo Old Town (COT) area zoning. As the plan is implemented over time, complementary modifications to the City's Zoning Ordinance may also be undertaken.

Infill Development Assistance

In order to achieve more unified development throughout the area, the City would collaborate with existing property owners and developers of large opportunity sites (e.g. Imation, EJM, Petit & Dawson) and smaller infill parcels (e.g. the narrow parcels along the railroad tracks) who are committed to preparing a cohesive plan for their reuse. The City will help facilitate the preparation of design plans that are consistent with the Dawson Drive Industrial Area Concepts and Design Guidelines. It would also coordinate the land use approval process to support development that meets both the City's and the property owners' goals.

Given the presence of numerous irregularly shaped, narrow lots along the railroad tracks and the desire to rehabilitate older structures of historical or aesthetic significance, the Agency would consider providing assistance to encourage infill development along the rail corridor. In addition, if a significant investment in public infrastructure is required in order to accommodate new development, the City would consider helping to defray extraordinary costs for public infrastructure.

SECTION 9

DESIGN GUIDELINES

- A. Introduction*
- B. Site Design & Planning*
- C. Building Design*
- D. Auto Repair & Service Shops*
- E. Environmental Sensitivity*
- F. North Dawson Drive
Railroad Retro*

A. INTRODUCTION

The purpose of this section is to:

- Communicate the design guidelines for Industrial/Mixed Use projects within the Dawson Drive area of the Camarillo Corridor Redevelopment Area.
- Facilitate the fair and consistent application of design objectives.
- Protect investment in the community by encouraging consistently high quality development.
- Foster a sense of community for owners and businesses in the area and community as a whole.
- Facilitate safe, functional and attractive development.
- Accommodate business functions in industrial parks while screening views from non-industrial uses.
- Encourage sustainable site and building design standards that conserve energy.



FIGURE 9-1
Area of Applicability for Design Guidelines

Applicability:

To achieve these purposes, the Guidelines apply to all new industrial, office, and mixed developments and to all development undergoing substantial alterations requiring approval by the City. These guidelines are also designed to assist private improvements within the Dawson Drive area not requiring City entitlements.

Use of Guidelines:

The provisions set forth in this document identify the desired level of design quality for industrial/mixed use development. However, flexibility is necessary to achieve excellent designs. To that end, the use of the words shall and must have been purposely avoided within the specific guidelines. Each application for industrial/mixed use development, however, should demonstrate to what extent it incorporates these guidelines.

Organization:

The guidelines are divided into three sections: Site Planning, Building Design and Environmental Sensitivity. Within each section are a number of design principles and measures that address the different elements of site and building design and environmental sensitivity.

B. SITE DESIGN AND PLANNING

Planning for development on a site encompasses items such as its relationship to surrounding uses, building orientation on the site, pedestrian and vehicular circulation, and efficiency of parking areas, screening of loading and utility areas, and the design of landscaping, signage and lighting.

1. Relationship to Adjacent Residential and Commercial:

- a) Encourage low-scale design and adequate buffering when industrial/mixed use developments are located adjacent to existing and future residential and commercial developments.
- b) Orient functional uses such as trash enclosures, compactors, truck loading areas and outdoor storage away from residential uses and views from the street to the extent practical. Consider views from elevated streets and the railroad as well.

2. Building Orientation:

- a) Orient office uses forward, toward the front of the site.
- b) Separate public/visitor areas from truck delivery and maneuvering areas.
- c) Orient and design new structures and additions for minimum solar gain, reflectivity and glare, and to achieve an optimum level of energy efficiency.



Large industrial uses present unique design challenges such as employee/visitor parking, truck loading and maneuvering, pedestrian connections, rain water detention, energy efficiency, etc.

3. Pedestrian and Employee Amenities and Hardscape:

- a) In large multi-building projects such as business or industrial parks, prepare master pedestrian circulation plans addressing connections between buildings and connections to supporting land uses in lieu of sidewalks on both sides of internal streets.
- b) In large multi-building projects, organize the site layout to provide functional employee spaces, including shade structures and amenities between or in front of buildings.
- c) Provide weather and sun protection, such as overhangs, awnings, canopies, etc. to mitigate climatic and solar conditions.
- d) Accessible parking spaces should be convenient to building entries.
- e) Where industrial/employment areas adjoin existing or proposed public transit routes, developers are encouraged to provide shaded, safe, well lighted and aesthetically pleasing bus stops.
- f) Provide convenient pedestrian access between bus stops and adjacent industrial/employment buildings.
- g) Disperse bicycle parking facilities throughout larger sites and locate them in convenient and visible areas in close proximity to primary building entrances.
- h) Provide for a continuation of pedestrian access when industrial developments are located adjacent to existing or planned open space.
- i) Design patios and pedestrian areas with architectural and landscape shade elements.

4. Vehicular Circulation and Parking:

- a) Encourage separation of heavy equipment traffic from employee traffic.
- b) Coordinate circulation and parking areas with adjoining sites.
- c) Locate parking at rear or interior side yard of buildings.
- d) Disperse parking into smaller fields instead of large paved areas, consider cooler paving materials, and shade via trees or shade structures.
- e) Avoid duplicating circulation routes whenever possible in favor of greater landscaped open space.
- f) Parking and utility screen wall design should follow the common exterior wall treatment for the industrial subdivision or be coordinated with the building design.
- g) Locate parking lot screen walls outside of required landscape areas and encourage screening by way of surrounding landscape
- h) Use canopy trees in parking lots to break up the scale of large parking lots, provide additional shading and reduce “heat island” impacts.
- i) Provide pedestrian access to and through parking areas, surrounding streets and linkages.
- j) Use water permeable parking surfaces

- k) Extend landscape planters into parking areas for water quality/storm runoff.
- l) Maintain opportunities for railroad access in industrial parks that are adjacent to the Union Pacific Railroad mainline.



Provide smaller parking areas with shade trees.



Water runs from sidewalk and parking surface into planter area.



Loading docks should not be oriented toward the street.

5. Loading Areas and Accessory Equipment:

- a) Service areas, loading and storage areas, and refuse enclosures should be oriented away from public street frontages or screened from these frontages.
- b) Design refuse enclosures with decorative masonry walls and sight-tight gates to match design features of the industrial development. Solid covers should provide a trellis design per City standard.
- c) Provide common refuse enclosures where possible.
- d) Where service electrical system (S.E.S.) panels are visible from off-site, recess panels into the building elevation and screen with doors, screen with landscaping or a solid wall (with landscaping) built of similar building materials and colors of the main development and equal to or exceeding the height of the S.E.S. panel.
- e) Locate ground-mounted utility cabinets out of the setback areas where they do not conflict with prominent site views or impair vehicle site distances and can be screened from major streets and public areas. Paint cabinets and screen walls to match the principal structure. Where space allows, provide landscaping in front of screening walls.
- f) Strive to ensure that ground-mounted utility equipment and cabinets are level and at the grade of the surrounding area.

6. Signage and Wall Design:

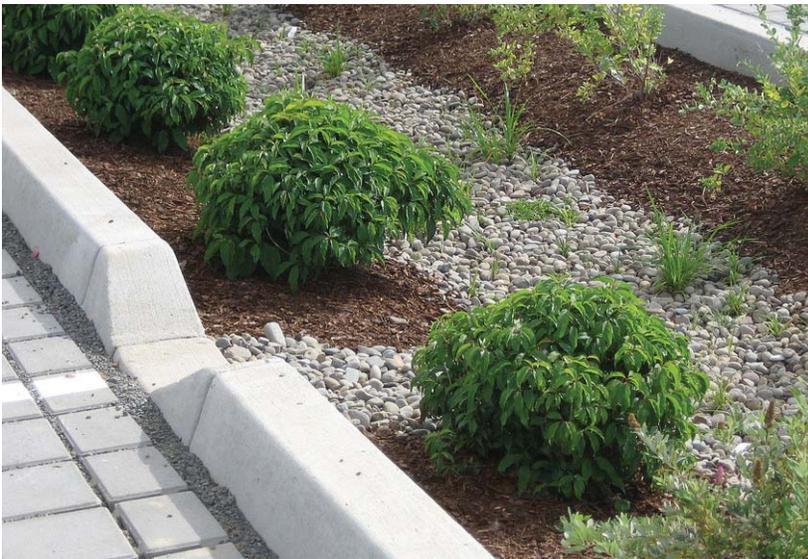
- a) New buildings would require master sign programs with review and approval by the City. The Fire Department would review design and location of address numbers.
- b) In large multi-building projects, coordinate signage design and materials, including building addresses, to create a consistent style of building identification.
- c) In large multi-building projects, street addresses should be provided at main entrances using directory signs. On individual buildings, address numbers should be readable from the street.
- d) In industrial subdivisions having no approved perimeter wall theme, developers of perimeter lots should use a consistent design on the outside of their rear walls that abuts or lies adjacent to a residential or lesser zone or are visible from arterial streets or open space.



Example of a Directory Sign.



Master planned landscaped retention area.



Drainage and water detention integrated with landscape.

7. Landscaping and Grading:

- a) In large multi-building projects, applicants should prepare master drainage plans addressing common storm water retention needs.
- b) Design retention basins that are visible from public streets and common open spaces to avoid a “bathtub” or linear channel appearance. Highly visible retention basins should be contoured using curvilinear design and landscaped with a combination of vegetative and non-vegetative materials.
- c) Design the project landscape theme to complement and enhance project architecture.
- d) As a general rule, low water use and drought tolerant trees and plants are preferred.
- e) Incorporate perimeter landscaping where sites border non-industrial uses.
- f) Design arterial street intersection frontage with substantial hardscape and landscape features, creative grading design, seasonal color, art and/or vertical landscape focal points. Incorporate “Welcome to Camarillo” features at designated gateway to the City and Dawson Drive area.
- g) For large multi-building projects, incorporate prominent entry features, vertical landscape forms and/or seasonal color at both vehicular and pedestrian project entrances appropriate to the scale of the project.

- h) Shade trees should be located throughout all paved parking areas wherever they are located on site and in association with pedestrian and employee amenities and gathering areas. Enclosed service, loading and storage areas are excluded.
- i) Provide significant foundation and/or accent plantings, including trees, around buildings to accentuate or screen building and parking structure elements.
- j) Provide low-profile accent plantings at the base of monument signs.
- k) In highly visible areas, use taller and larger caliper trees.
- l) Use predominately deciduous shade trees adjacent to west, south and southwest exposures to cool these elevations in the summer yet allow heat gain in the winter.
- m) For the overall site, use a mixture of deciduous and evergreen trees.
- n) If retaining walls are necessary in retention basins, they are to be terraced and landscaped to reduce their visual scale.
- o) Coordinate landscaping plans with above and below ground utility locations.
- p) Generally, provide a minimum of a 15 foot wide landscape strip along all public street frontages of projects. This may vary depending on street type eg) narrower on Dawson Drive and wider on Lewis Road.



Landscaped entryway.



Combine fences and berms to reduce the apparent height of fencing.



Use common local landscape species to provide shade and color.

- q) To provide a visual transition between railroad tracks and industrial buildings, a minimum 10 foot wide landscaping strip is to be provided along the frontage on railroad tracks.
- r) When industrial buildings are located adjacent to residential uses, provide a minimum 10 foot wide landscape strip with a minimum 8 foot high masonry wall.
- s) Provide a minimum 5 foot wide landscape strips along the sides and rear property line of all industrial sites.
- t) In designing the perimeter landscaping, street trees and sidewalks are to be taken into consideration and incorporated into the design.
- u) Use a combination of trees, shrubs and ground cover between the buildings and public right-of-way. Water wise ground covers are highly encouraged in lieu of grass. In highly visible areas use annuals or other non-drought tolerant plant material minimally.
- v) Combine street trees with on-site trees to provide a double row of trees where possible.
- w) When perimeter landscaping is used to screen a development, combine fences and berms to reduce the apparent height of fencing that is needed.

8. Lighting:

- a) All lighting shall conform to City lighting standards/policy.
- b) Provide pedestrian scale lighting fixtures adjacent to pedestrian paths and employee amenities. Select lighting fixtures that complement the general architectural style of the development or sub-area.
- c) Highlight significant architectural features, specimen trees and artwork with accent lighting. Lighting an entire building or major portion thereof is discouraged.
- d) Provide security lighting that is both effective and attractive to promote a safe and secure facility.



Provide pedestrian scale light fixtures adjacent to pedestrian paths and employee amenities.

C. BUILDING DESIGN

1. Design:

These Guidelines do not dictate a design theme other than for the North Dawson Historic Railroad area. See Section F for guidelines related to the Historic Railroad area.

The intent of the architectural guidelines is to ensure a base level of quality architecture that is responsive to its context and builds upon the aesthetic identity of the community rather than a design solution that is based on a standardized formula or market prototype superimposed on the selected site. Over time, certain projects and landmark buildings begin to define the dominant character of an area. Not all buildings within an area contribute equally to the area character and each example should be weighed against the balance of all other projects. The intent of the architectural guidelines is to encourage proposals that fit within and contribute to the established or planned architectural character and context of a specific area.

- a) All elevations generally visible from public view should reflect the overall design, colors and textures used on the front façade.
- b) Design multi-building projects to include consistent design elements throughout the project.
- c) Fully screen roof mounted mechanical equipment to ensure that the top of equipment is below the top of the equipment screen / parapet.
- d) Internalize roof drain elements within the building or apply an architectural feature where visible from streets and public areas.
- e) Predominant exterior building materials shall be of high quality, energy efficient and durable. These include, but are not limited to:
 - Brick.
 - Stone, natural or faux.
 - Integral color, sand blasted or stained textured masonry.
 - Split-face or scored concrete masonry units.
 - Textured tilt-up concrete panels.
 - Stucco/Exterior Insulation and Finish Systems (EIFS).
 - Metal roofs.
 - Concrete and clay tile roofs.
 - Light colored or reflective "Cool roofs".
 - Clear glass.
 - Architectural metal.
 - Prefabricated steel panels where architecturally integrated.
- f) Building trim and accent areas may feature contrasting building materials and different colors than the building field color.

- g) Buildings should have clearly defined public and employee entrances incorporating a combination of elements such as:
- Canopies or porticos.
 - Overhangs.
 - Recesses/projections.
 - Arcades.
 - Raised corniced parapets over the door.
 - Peaked roof forms.
 - Tower Elements
 - Arches.
 - Entrance framed by outdoor pedestrian features or enhanced landscaping.
 - Architectural details such as tile work and moldings integrated into the building structure to frame the entryway.
 - Integral planters or wing walls that incorporate landscaped areas and/or sitting areas.
 - Enhanced pedestrian surfaces.

2. Freestanding Accessory Structures:

Enclosed service/refuse areas and covered parking should be designed to be an integral part of the building architecture. The forms, colors, textures and materials used on the main building should be applied to all sides of these structures when visible to the public.



Architectural details and enhanced landscape improve the appearance of accessory structures.

D. AUTO REPAIR AND SERVICE SHOPS:

- a) Buildings and sites should be oriented to prevent direct public view into the service and repair areas. The office, store, and reception areas should be located on the public street side with work bays on the sides or at the rear of the building.
- b) All work areas facing public streets or adjacent properties are to be screened from view by landscaping and/or solid walls.
- c) Outdoor storage areas for junk and dismantled cars and parts should be discouraged because they cannot be screened from elevated views.
- d) On-site circulation drives and parking shall adequately serve the project's need to avoid interference with traffic flow on adjacent public streets.



Carport with solar panels.

E. ENVIRONMENTAL SENSITIVITY:

The following measures that promote environmental sensitivity and potential long-term cost savings are offered for consideration by the development community:

- Orient and design new structures and additions for minimum solar gain, reflectivity and glare, and to achieve an optimum level of energy efficiency.
- Shelter entries and windows and use architectural shading devices and landscaping to minimize cooling losses.
- Use energy efficient materials in doors and windows.
- Use energy efficient lighting.
- Mitigate urban heat island effects with cool roofing materials, shade trees and cool paving materials. EPA Website: www.epa.gov/heatisland/strategies
- Reference national programs for environmentally sensitive development methods such as Leadership in Energy & Environmental Design (LEED), International Energy Conservation Code (IECC) and Energy Star Labeled Buildings. Websites: www.usgbc.org/leed and <http://www.iccsafe.org/cs>
- Encourage the integration of solar panels on roofs and carports.

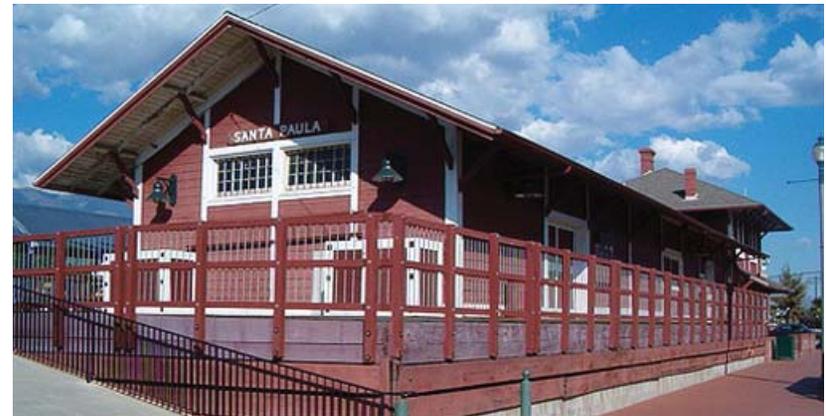
F. NORTH DAWSON DRIVE - HISTORIC RAILROAD

Consistent with the Heritage Zone, the North Dawson subarea is envisioned to be developed/redeveloped with a design theme reminiscent of turn of the century railroading. Craftsman style or modern interpretations of Craftsman style would be appropriate within the Heritage Zone and the North Dawson subarea. The most important aspect of the Heritage Zone is the type of materials, their colors and textures, and the scale of the architectural elements within the building design. The types of building elements and materials appropriate for the North Dawson subarea would include:

- Large elevated and covered loading areas.
- Large sliding doors.
- Multi-paned windows.
- One and two story elements.
- Standing seam or shingle roofing.
- Roof vents.
- Industrial era lighting.
- Brick, v-rustic, or board and bat siding.
- Deep set eaves.



Metal Roofing, such as Standing Seam.



Elevated loading area, large sliding doors, one and two story elements, and deep set eaves.



Gooseneck-style industrial lighting.



Shed style of architecture.

SECTION 10

IMPLEMENTATION ACTION PLAN

In order to implement the proposed Dawson Drive Industrial Area Concepts and Design Guidelines, the City will need to adopt zoning and other regulatory measures to improve the appearance, cohesiveness and functionality of the area. In addition, the City and the Redevelopment Agency (Agency) will need to invest public funds in infrastructure improvements and encourage property owners and developers to upgrade existing properties and invest in infill development.

This section describes the proposed components of the Agency's Implementation Action Plan that is designed to help the City achieve the eight revitalization objectives described in Section 2:

- Strengthen the Dawson Drive Industrial Area so it can better serve local residents and businesses, as well as attract new visitors to central Camarillo
- Create a sense of place, capitalizing on its central location, transit access and history
- Enhance existing neighborhood fabric
- Improve access, signage and visibility
- Upgrade vehicular, pedestrian and bicycle circulation to more seamlessly connect the Dawson Drive Area with other key parts of central Camarillo
- Promote transit use and access along the North Dawson Drive rail corridor
- Encourage infill development and redevelopment of properties
- Expand private investment and business growth



IMPLEMENTATION ACTION PLAN COMPONENTS

The implementation of the Dawson Drive Industrial Area Concepts and Design Guidelines will occur over time. Adoption of these Guidelines will represent a commitment by the City to realize a vision of a revitalized Dawson Drive area over the medium to long term. In order to achieve this vision, the implementation action plan must be flexible and able to respond to the local economy, the available funding sources, and the evolving preferences of the Camarillo community.

In general, the implementation of the revitalization programs described throughout this document will require the strategic use of regulatory actions, funding sources and related implementation measures. Table 10-1, Implementation Action Plan Components for the Dawson Drive Industrial Area, summarizes the recommended steps to achieve the City's vision. Each action step is organized according to the five core program components (described in greater detail in the report sections referenced in parenthesis):

- Circulation and Transit- vehicular, pedestrian, bicycle and transit improvements (Section 5)
- Placemaking- enhancing the area through improved land use, design and planning guidelines, signage, gateway treatments, and landscaping (Section 6)
- Infrastructure- utility improvements to encourage infill development (Section 7)

- Economic Development and Neighborhood Enhancement- programs to retain, expand and attract new businesses and to enhance neighborhood fabric and encourage redevelopment and infill development of properties (Sections 8)

Action Step Priorities

The action plan is prioritized according to the likely timing for each action step that would occur within the five program categories:

- Short-range (1st year after adoption)
- Mid-range (2–5 years)
- Long-range (5+ years)

As the program will occur over time, the regulatory actions and funding mechanisms will evolve as development progresses. However, Table 10.1 is designed to serve as an initial blueprint for the way implementation could occur.

Funding Sources

In order to undertake the desired Dawson Drive Industrial Area improvements, a combination of local, state, federal and private funding sources will need to be deployed in order to undertake each of the recommended action steps. Table 10.1 also includes the primary funding sources that would likely be used to help accomplish each action step. A description of each key funding source along with its controlling agency, use, and limitations is presented in Appendix A.

Conclusion

This report presents a vision for the revitalization of the Dawson Drive Industrial Area, a recommended set of programs that are designed to achieve this vision, and an implementation action plan that describes how these programs may be undertaken over time. It is designed to serve as a blueprint for future action by the City of Camarillo and the Redevelopment Agency, in a collaborative revitalization effort with area businesses, property owners and developers.

TABLE 10-1
Implementation Action Plan Components

Improvement	Priority	Action	Comments	Potential Funding Sources ^a
Circulation and Transit				
Dawson Drive North: street improvements	Mid-Range	Provide complete street section including bike lane/bike path per city design standards.	Because of narrow ROW, increased sidewalk widths may be needed to accommodate ADA accessibility.	Tax Increment, General Fund and CIP, STP, TDA
Dawson Drive South: street improvements	Mid-Range	Develop landscape plan. Bike Lanes/Sidewalks included in CIP.	Because of narrow ROW, increased sidewalk widths may be needed to accommodate ADA accessibility.	Tax Increment, General Fund and CIP, STP, TDA
Calleguas Gardens: traffic calming/sign program	Mid-Range	Add traffic control measures to CIP. Develop neighborhood signage program.	Consider bulb-outs and/or speed humps as part of CIP. Develop neighborhood signage program and entry treatment.	General Fund and CIP, Fuel Tax
Pedestrian and bicycle network	Mid to Long-Range	Develop pedestrian and bicycle network and add to CIP.	Pedestrian and bicycle network should be incorporated into all future projects and modifications within the project area.	General Fund and CIP, Tax Increment
Provide and incorporate way finding program	Mid to Long-Range	Develop way finding sign program, and include in CIP.	Develop way finding program for Dawson Drive circulation area.	General Fund and CIP, Tax Increment
Petit Street: widening	Long-Range	Widening accomplished through City capital improvement plan.	Identify preferred street alignment. Underground overhead utilities. Widen/provide sidewalks, curbs and gutters.	General Fund and CIP, Tax Increment, Development Impact Fees, Fuel Tax, STP, CMAQ, STIP

Improvement	Priority	Action	Comments	Potential Funding Sources ^a
Circulation and Transit cont'd				
Lewis Road: parkways	Long-Range	<p><u>West Side:</u> Provide screen walls and enhanced landscaping.</p> <p><u>East Side:</u> Provide bike path and greenbelt along I-405 and EJM properties.</p>	<p>Enhance parkways on west side of Lewis to screen industrial properties.</p> <p>Green belt on the east side of Lewis Road to be accomplished at time of development of adjacent properties.</p>	General Fund and CIP, Tax Increment, Development Impact Fees, STP, TDA, ISRF
Railroad Crossing at grade (preferred) -pedestrian crossing to MetroLink, parking and Old Town	Long-Range	Conduct study to determine appropriate traffic control system for at-grade pedestrian crossing.	According to VCTC, this option would require a second track.	Tax Increment, CMAQ, STIP, TDA
Elevated railroad crossing (alternative)	Long-Range	Enhance existing elevated crossing with additional stairway or construction of new elevated crossing from Metrolink platform on south side of freeway.	Additional stairway from Metrolink platform, enhancements to existing pedestrian bridge and/or new elevated crossing in conjunction with new Metrolink shelter.	Tax Increment, CMAQ, STIP, TDA
Dawson at Lewis Road bridge: develop parking/ landscape plan for "triangle island"	Long-Range	Add hardscape construction to CIP. Develop landscape/parking plan and install entry monuments.	This area will announce "South Dawson Drive Industrial Area" to southbound motorists and "North Dawson Drive" to northbound motorists.	General Fund and CIP, Tax Increment, Development Impact Fees

Dawson Drive Industrial Area Concepts and Design Guidelines

Improvement	Priority	Action	Comments	Potential Funding Sources ^a
Placemaking				
Lewis Road: screening	Short-Range	Provide screen wall along Lewis Road at rear of industrial properties.	To be installed in conjunction with enhanced landscaping as part of CIP.	General Fund and CIP, Tax Increment, Developer and Property Owner Contributions
Street signing plan	Short-Range	Develop sign program to identify unique areas.	Provide street signs to identify unique areas. Develop street sign plan for Dawson Drive area.	General Fund and CIP, Tax Increment
Landscaping (onsite)	Short to Long-Range	Work with individual property owners to develop plans consistent with city standards. OPA may be a tool to enhance front yard landscaping where desirable for overall area enhancement.	Landscape enhancement and development will be ongoing with private on-site development and redevelopment subject to city review and approval.	Tax Increment, Developer and Property Owner Contributions
Street lighting plan	Mid-Range	Develop design alternatives for unique area lighting. Install with area street improvements.	Consider “off the shelf” retro lighting provided by SCE for Calleguas Gardens neighborhood. Extend Old Town lighting into North Dawson Drive area.	Tax Increment, General Fund and CIP
Gateways	Mid to Long-Range	Develop design alternatives for gateway monumentation.	Ongoing: gateways will be provided as part of street improvements (CIP) and through private development.	Tax Increment, Developer and Property Owner Contributions
Street Furnishings	Mid- to Long-Range	Street furnishings program including trash cans, recycle bins, benches, etc. to be accomplished in conjunction with street improvements.	North Dawson area may include extension of Ventura Boulevard street furnishings. South Dawson may provide a more industrial look.	Tax Increment, Developer and Property Owner Contributions, BID

Dawson Drive Industrial Area Concepts and Design Guidelines

Improvement	Priority	Action	Comments	Potential Funding Sources ^a
Infrastructure				
Water	N/A	Adequate for existing.		Development Impact Fees, Tax Increment, Developer Contributions
Sanitary Sewer	N/A	Adequate for existing.		Development Impact Fees, Tax Increment, Developer Contributions
Natural Gas	N/A	Adequate for existing.		Tax Increment, Developer Contributions
Drainage (on site)	Short- to Long-Range	Work with property owners to determine BMPs for storm water retention and to minimize impacts of storm water runoff consistent with current Ventura County Storm water permit.	On-site improvement will require consistency with Ventura County storm water permit.	General Fund and CIP, Development Impact Fees, Developer and Property Owner Contributions, ISRF
Drainage/Storm Drain modifications (offsite)	Mid-Range	Provide improvements to storm drain system to alleviate drainage problems in the area identified in study as part of CIP.	Certain properties within the Dawson Drive Area have been identified as having flooding/ponding issues.	General Fund and CIP, Development Impact Fees, Developer and Property Owner Contributions, ISRF
Underground overhead utilities	Mid- to Long-Range	Underground utilities as part of CIP and/or private development or redevelopment.	Consider creation of an Undergrounding District to consider the needs of individual smaller properties.	General Fund and CIP, Tax Increment, Developer and Property Owner Contributions, Rule 20

Improvement	Priority	Action	Comments	Potential Funding Sources ^a
Economic Development				
Façade and Property Improvements	Ongoing	As individual property owners request city assistance, an Owner Participation Agreement may be considered.		Tax Increment, Property Owner Contributions
Business Assistance	Ongoing	Work with consultant to determine what business assistance programs are available and desirable for the area.	Assistance would enhance what is currently available to all businesses in Camarillo based on the unique nature of the Dawson Drive area.	Tax Increment, BID
Code Compliance	Short-Range	Identify properties with violations that can be corrected through code compliance action.		General Fund, Tax Increment
Create overlay zone for South Dawson	Short-Range	Develop overlay zone for South Dawson Drive to assist existing uses and possible future uses.	Determine which business practices currently non-compliant with municipal code are acceptable for the area and include in overlay zone.	General Fund
Billboard Removal	Short-Range	Seek removal of billboard through code compliance or redevelopment actions.		Tax Increment

Dawson Drive Industrial Area Concepts and Design Guidelines

Improvement	Priority	Action	Comments	Potential Funding Sources ^a
Neighborhood Enhancement				
Enhance existing neighborhood	Short-Range	Collaborate with local businesses and property owners to undertake proposed improvements. Adopt recommended design guidelines.	Improvements include neighborhood identification signs, traffic calming measures, sidewalk improvements, screening and buffers.	General Fund and CIP, Tax Increment, Developer and Property Owner Contributions
Encourage infill mixed use development along rail corridor	Short-Range	Re-zone North Dawson Drive area		Tax Increment, Developer and Property Owner Contributions, LIHTC
Infill Development Assistance	Ongoing	Work with property owners and developers to determine preferred and best uses of properties. Determine if assistance is available and/or desirable.	Hire a consultant to assist with business assistance.	Tax Increment, Developer and Property Owner Contributions

^aSee Appendix A for a full description of each potential funding source.

KEY TO ACRONYMS:

STP	Surface Transportation Program	ISRF	Infrastructure State Revolving Fund
CMAQ	Congestion Mitigation and Air Quality Improvement Program	BID	Business Improvement Districts
STIP	State Transportation Improvement Program	IIG	Infill Infrastructure Grant Program
TDA	Transportation Development Act	TOD	Transit-Oriented Development
		LIHTC	Low Income Housing Tax Credits

APPENDIX A

FUNDING SOURCES

As described in Chapter 10, a combination of local, state, federal and private funding sources will be utilized to accomplish the recommended implementation action plan. This appendix presents a brief overview of key funding sources, accompanied by a table that describes each funding source, including each source's applicability and limitations.

FUNDING SOURCES

Redevelopment Tax Increment

The Dawson Drive Industrial Area is within the Camarillo Corridor Redevelopment Project Area (Project Area). Thus, the Agency could use tax increment to help fund public infrastructure and encourage local property owners, businesses and developers to invest in the area. Tax increment revenue is generated from increases in property taxes that occur over time, as assessed values in the Project Area grow, compared to assessed values at the time when the redevelopment plan was adopted. As the Dawson Drive area represents only a portion of the Project Area, and other subareas like Old Town Camarillo also need tax increment investment, not all of the Agency's tax increment would be available to help fund the Dawson Drive area improvements.

Tax increment is a flexible capital funding source that can be used to help address all of the components of the proposed Dawson Drive area improvements. All expenditures of tax increment revenues must either contribute to the elimination of blighting conditions within the Project Area, or increase, improve or preserve the supply of affordable housing in Camarillo.

Tax increment revenues may be spent on a pay-as-you-go basis or may be pledged to issue bonds for major improvements. Given the scale of the proposed improvements in the Dawson Drive area, a strategy that combines the two approaches is most appropriate. The amount of tax increment revenue available to fund the Dawson Drive area, will depend on growth in assessed values within the overall Project Area, and the Dawson Drive portion in particular. In order to help fund improvements for the Dawson Drive area, the

Redevelopment Agency would need to allocate tax increment through its annual Agency budgeting process and ensure that its programs comply with the Agency's Five-Year Implementation Plan.

A key goal of redevelopment activities within the Dawson Drive area is to spur private development and associated property-value increases through initial investments in catalyst projects. The faster new development can be catalyzed, the more local revenues can be generated and invested in the Dawson Drive area, creating a "virtuous cycle" of investment. Therefore, leveraging a variety of funding sources beyond tax increment is crucial for the successful redevelopment of the Dawson Drive area.

Development Impact Fees

The City has a comprehensive development impact fee program that charges fees on new development to help fund public improvements, that are needed as a result of new development, namely bridges, traffic mitigation, water and sewer utilities, and the undergrounding of electrical utilities. Like tax increment, development impact fee revenues depend on the pace of new development in the City, which has slowed in recent years. Some citywide development impact fee revenues may be available to help fund high priority Dawson Drive improvements in the near term, such as the proposed circulation and utility improvements. This would be accomplished through a budget allocation in the City's Five Year Capital Improvement Program. Alternatively, future impact fees generated by new development within the Dawson Drive Industrial Area could be used to support future improvements related to those developments.

General Fund Revenues

The City's General Fund receives revenues from a variety of sources, with the primary sources being sales tax, transient occupancy tax, property taxes and statewide fund transfers, such as gas tax revenues to help improve streets. General Fund revenues may be transferred and allocated through the City's Capital Improvement Program to finance major capital improvements. General Fund revenues may be used in a flexible manner, but as they are used to fund both general City government services and citywide capital improvements, the funds available for Dawson Drive area improvements will likely be limited. As with tax increment and development impact fees, the City would allocate funds through the annual budgeting or capital planning process to fund initial high-priority catalyst projects.

Land Secured Financing Districts

The City could help create financing districts to fund public infrastructure and related maintenance that are secured by the property that will be improved. Land-secured financing mechanisms, including Mello Roos Community Facilities Districts and Assessment Districts such as Landscape and Lighting Districts, have all been used elsewhere in Camarillo. These districts allow property owners to elect to publicly finance improvements over time, through the payment of special taxes or assessments on property. Depending on the structure of the established districts, these financing mechanisms can fund infrastructure improvements associated with specific parcels, or they can provide an ongoing source of funds for operations and maintenance.

Owner Participation and Developer Contributions

Property owners and developers are crucial partners in the funding and realization of the Dawson Drive Industrial Area revitalization. Owners and developers will be encouraged to comply with the Concepts and Design Guidelines as a condition for project approval. Contributions could include commitments to improve the facades of properties, upgrade buildings or invest in circulation and infrastructure improvements directly related to a particular property. Owners and developers will benefit from their contributions through enhanced property values and the unified growth of the Dawson Drive area. The City and/or Agency may enter into owner participation agreements or impose development conditions, to ensure that developer contributions are commensurate with the need and the benefit to be provided.

Business Improvement Districts

The businesses and property owners within the Dawson Drive Industrial Area may elect to form a Business Improvement District (BID), which would use the proceeds of a special assessment to fund a variety of enhanced services and physical improvements. A BID is an opportunity for property owners and businesses to work together to achieve a shared vision of a vibrant Dawson Drive area, contributing ideas and resources to its revitalization. The Redevelopment Agency could help seed the formation of the BID as well as help fund its initial operations, as envisioned in Old Town Camarillo.

State Programs

The State of California has a variety of programs that provide grant or financing assistance for transportation, infrastructure, economic development and housing projects. These programs are primarily administered through the California Transportation Agency (Caltrans), California Infrastructure and Economic Development Bank (IBANK), and the California Department of Housing and Community Development (HCD). Often regional organizations, such as the Southern California Association of Governments (SCAG) and the Ventura County Transportation Commission (VCTC), are responsible for setting priorities, allocating and/or administering these funds.

The current State programs that are most likely to be applicable to the Dawson Drive Industrial Area are the State Transportation Improvement Program (STIP), the Transportation Development Act (TDA), fuel tax revenues, the Infrastructure State Revolving Fund (ISRF), the Infill Infrastructure Grant (IIG) program (IIG), and the Transit-Oriented Development (TOD) program. The accompanying table describes these programs in more detail. Many of these programs are funded by statewide general obligation bond proceeds or annual appropriations by the State Legislature, making their availability in times of State budget crisis uncertain. These State programs often change or are terminated in response to evolving statewide policy priorities, budget constraints and voter initiatives.

Federal Programs

The Federal Government has a variety of programs that could help fund improvements in the Dawson Drive Industrial Area. Currently, the largest source of federal funds is transportation funding that is currently authorized under the Safe, Accountable, Flexible and Efficient Transportation Equality Act (SAFETEA). SAFETEA funding is administered by the U.S. Department of Transportation (DOT), and some of the funds are allocated regionally through VCTC. The accompanying table describes several relevant components of SAFETEA, including the Surface Transportation Program (STP) and the Congestion Mitigation and Air Quality Improvement Program (CMAQ). While SAFETEA expires this year, based on historical experience this program or a similar transportation funding program, will likely be reauthorized with a greater emphasis on transit related funding.

In addition, the Federal Government also has a variety of grant and loan programs that are administered through the U.S. Department of Housing and Urban Development (HUD), the Environmental Protection Agency (EPA) and the Economic Development Administration (EDA), but typical grant amounts are much lower than awards through the DOT. For example, Camarillo currently receives Community Development Block Grants through HUD, but these limited funds are committed to citywide housing and community development activities.

The following table describes each of these programs and their applicability and limitations.

Source	Agency	Description	Applicability	Limitations
City of Camarillo and its Redevelopment Agency				
Redevelopment Tax Increment	City of Camarillo Redevelopment Agency	Revenue generated by the increase in property values within the Camarillo Redevelopment Project Area. Eligible activities include those that contribute to the elimination of blighting conditions (including economic development and public improvements) within the Project Area or to the creation of affordable housing. Funds may be allocated on a pay-as-you-go basis or may be pledged to issue bonds.	Redevelopment Tax Increment is a flexible funding source that may address a range of needs not covered by other financing mechanisms. Typical tax increment-funded initiatives include loans to small businesses for facade improvements and business development and investment in street and sidewalk improvements via tax allocation bonds.	May only be used to eliminate existing documented blight. Available revenue depends on how the Agency prioritizes among its redevelopment projects and how quickly property values within Dawson Drive increase as a result of initial investments. The Agency must issue bonds before the redevelopment plan's debt incurrence limit.
Development Impact Fees	City of Camarillo	Fees on new private development to mitigate specific consequences of growth. Under applicable state laws, such fees can be imposed on a new private development only to the extent that a direct nexus or relationship exists between the need for public facilities caused by such new development and the level of fees imposed. Development impact fees can cover only the portion of the cost of needed public improvements attributable to new development.	Existing City of Camarillo development impact fees include fees for bridges, traffic mitigation and undergrounding utilities including water and sewer facilities. Revenues can be used for relevant Dawson Drive area improvements.	Revenues only generated as new development occurs, so recent slowdown in new development has reduced funding. Dawson Drive must compete with other City infrastructure priorities for funds. Any new fees would require a nexus study to comply with state law.

Dawson Drive Industrial Area Concepts and Design Guidelines

Source	Agency	Description	Applicability	Limitations
City of Camarillo and its Redevelopment Agency Cont'd				
General Fund and CIP	City of Camarillo	Revenue generated from a variety of sources, including sales taxes (including sales taxes on business-to-business sales), transient occupancy tax (TOT), property taxes, and other sources.	General Funds can be used for a variety of operational and capital funding purposes and are allocated by the City Council.	Many City priorities compete for General Fund revenues, so resources available for Dawson Drive improvements likely to be limited. The City annually allocates funding for capital improvement projects through its Capital Improvement Program (CIP).

Source	Agency	Description	Applicability	Limitations
Circulation and Transit				
SAFETEA - Surface Transportation Program (STP)	U.S. Dept. of Transportation, Ventura County Transportation Commission (VCTC)	flexible grant funds to be used on surface transportation projects. At least 10 percent of STP funds must be spent on projects that enhance transportation systems such as scenic beautification, historical preservation, and bicycle and pedestrian facilities. Other uses include road improvements, transportation system management and operations, and environmental restoration. A portion of the STP apportionment is set aside for Transportation Enhancement (TE) projects. After the TE set aside, 62.5 percent of the remainder is distributed as Regional Surface Transportation Program (RSTP) funding, based on population, among the urbanized and non-urbanized areas of the State through Metropolitan Planning Organizations (MPOs) and Regional Transportation Planning Agencies (RTPAs).	STP funds could be used to fund the reconfiguration of Petit, streetscape improvements along Petit, North Dawson and Lewis Road, and a new bicycle/pedestrian path. Recent Camarillo projects funded by STP include the Lewis Road widening.	Funds are competitive and allocated by the Ventura County Transportation Commission. Program expires in 2009 and has yet to be reauthorized.
SAFETEA - Congestion Mitigation and Air Quality Improvement Program (CMAQ)	U.S Dept. of Transportation, Ventura County Transportation Commission (VCTC)	Grant funds for projects and activities that reduce congestion and improve air quality, both in regions not yet attaining federal air quality standards and in those engaged in efforts to preserve their attainment status.	CMAQ funds could be used to fund street improvements, such as the Petit widening and re-configuration, that reduce traffic congestion. Recent Camarillo projects funded by CMAQ include the Camarillo Station improvements.	Typically requires a local match of at least 11.47 percent of the total project cost. Program expires in 2009 and has yet to be reauthorized.
SAFETEA - Transportation and Community and System Preservation Program (TCSP)	U.S Dept. of Transportation, Southern California Association of Governments (SCAG)	Planning grants, implementation grants, and research to investigate and address the relationship between transportation and community and system preservation. Grants should plan and implement strategies that improve the efficiency of the transportation system, reduce environmental impacts of transportation, reduce the need for costly future public infrastructure investments, ensure efficient access to jobs, services and centers of trade, and examine development patterns and identify strategies to encourage private sector development patterns that achieve these goals.	TCSP planning grants can be used in the initial stages of implementing the Dawson Drive area plan. Additional resources could fund transit and general circulation improvements.	Grants are competitive and funding amounts are typically small.

Dawson Drive Industrial Area Concepts and Design Guidelines

Source	Agency	Description	Applicability	Limitations
Circulation and Transit cont'd				
State Transportation Improvement Program (STIP)	California Transportation Commission & Ventura County Transportation Commission (VCTC)	Grant funds for capital improvement projects approved by the California Transportation Commission (CTC) to be funded with State transportation funds, including proceeds from bond acts (such as Proposition 116) and motor vehicle fuel taxes. Also includes Federal funds apportioned to the State for transportation purposes. Projects that enhance quality of life in or around transportation facilities are eligible for additional funding.	STIP funds could be used to fund major projects such as the reconfiguration of Petit.	Funds are competitive and projects must be approved by both VCTC and CTC.
Transportation Development Act (TDA)	Ventura County Transportation Commission (VCTC)	Transportation grant funds from revenues generated statewide through a one-quarter-cent tax on retail sales in each county. The City receives an annual TDA apportionment, and the VCTC determines the ways in which the funds are spent. TDA funds may be used for regional and municipal transit projects, special transit projects for disabled persons, bicycle and pedestrian purposes, and other improvements or programs designed to reduce automobile usage.	TDA funds could be used to fund bicycle and pedestrian improvements in the Dawson Drive area.	VCTC must approve all projects.
Gas Tax	City of Camarillo	Revenues from a tax on gasoline, aircraft jet fuel and diesel fuel sales, as well as fuel use. These revenues may be used for street maintenance, construction activities and circulation improvements. Approximately one-third of the gasoline, diesel fuel and use fuel tax revenues are distributed to local jurisdictions on a formula based on population and other factors.	Gas tax funds could be used for planned street improvements and ongoing maintenance.	Funds are allocated among City projects, so Dawson Drive would compete with other City priorities.

Source	Agency	Description	Applicability	Limitations
Infrastructure				
Rule 20 Program	Southern California Edison (SCE)	Matching funds for the undergrounding of overhead electrical wires as well as other utilities. Rule 20A projects must occur in areas of public use, with an unusually high concentration of overhead facilities, or along arterial or collector streets. The work is carried out by SCE and paid via future electric rates system-wide. Property owners are responsible for connecting the undergrounded services to the building. Rule 20B and 20C services are also available, whereby SCE performs the undergrounding but with much less subsidy. The City must pay for the balance of improvements under Rules 20B and 20C through General Fund or special assessment districts voted upon by the property owners according to standard assessment district rules.	Rule 20 funds could be used to fund undergrounding of utilities in the Dawson Drive area.	Projects must be nominated as high priority by the City. Once approved, there may be a significant wait for services due to high demand.
Public Works Grants	U.S. Economic Development Administration (EDA)	Grants to support the construction or rehabilitation of essential public facilities and infrastructure necessary to generate private sector jobs and investment. Projects must also contribute to the long-term economic development of the area by creating or retaining permanent jobs and raising income levels. Eligible activities include infrastructure development such as water and sewer facilities, industrial access roads, rail spurs, port improvements, skill-training centers, technology-related infrastructure, as well as the demolition, renovation and construction of publicly owned buildings.	Public works grants could be used for road improvements or infrastructure for the redevelopment of major properties.	Application process involves several stages. Most planned Dawson Drive improvements may not be eligible.

Source	Agency	Description	Applicability	Limitations
Infrastructure cont'd				
Infrastructure State Revolving Fund (ISRF)	California Infrastructure and Economic Development Bank (IBANK)	<p>Low cost loans to public agencies for a wide variety of infrastructure projects with loan terms of up to 30 years to be repaid with local tax revenues. The interest rate is fixed for the term of financing and is set at 67 percent of tax-exempt "A" rated bonds with a weighted average life similar to IBANK financing. Eligible applicants include cities, counties, special districts, assessment districts, joint powers authorities and redevelopment agencies. Eligible projects include city streets, county highways, state highways, drainage, water supply and flood control, educational facilities, environmental mitigation measures, parks and recreational features, port facilities, public transit, sewage collection and treatment, solid waste collection and disposal, water treatment distribution, defense conversion, public safety facilities, and power and communication facilities.</p>	<p>ISRF funds could be used for road improvements, drainage, or environmental mitigation.</p>	<p>Funds would need to be repaid out of tax increment revenues and are not a direct source of funding. Most planned Dawson Drive improvements may not be eligible.</p>

Source	Agency	Description	Applicability	Limitations
Infrastructure cont'd				
<p>Mello Roos Community Facilities Districts (CFD)</p>	<p>City of Camarillo</p>	<p>Project funds are raised through special taxes used to fund capital improvement projects or operations and maintenance within a special tax district. Taxes are levied on real property and collected on the county property tax bills. A CFD is created by a sponsoring local government agency, and requires approval by two-thirds of residents living within the proposed boundaries or, if fewer than 12 voters, a vote of current land owners. The land owner vote is weighted based on the amount of land each owns, and two-thirds support is required for approval. After approval, a lien is placed against each property in the CFD, and property owners pay a special tax. The sponsoring agency can issue Mello-Roos bonds secured by the special taxes to finance public infrastructure or finance infrastructure on a “pay-as-you-go” basis. The taxes will continue until at least the infrastructure is paid for and/or bonds are repaid. At such a point, taxes will either be discontinued or lowered and used to maintain improvements.</p>	<p>Like assessment districts, CFDs are most frequently formed to pay for infrastructure in previously undeveloped areas. However, they can also be used in established communities, as long as they secure the votes needed for CFD formation. A CFD in the Dawson Drive area would likely finance basic infrastructure. A CFD is one of the few funding sources that can pay for operations and maintenance costs.</p>	<p>A CFD would have to be approved by two thirds of Dawson Drive landowners or residents. The nature of the improvements planed for Dawson Drive may not lend themselves to the establishment of a CFD.</p>

Source	Agency	Description	Applicability	Limitations
Economic Development				
Business Improvement Districts (BID)	Private Sector	Raises funds through assessments on property owners and/or businesses within set boundaries to pay for enhanced services, such as maintenance, security, marketing, economic development, parking and special events. BIDs are specialized assessment districts that establish a partnership between property owners and businesses for the purpose of improving the business climate in a defined area. The most common type of BIDs provide for an additional assessment of commercial property, which requires support from business and the creation of an advisory committee of property and business owners. BIDs are created and funded with the approval of assessees.	BIDs can fund enhanced services including maintenance, sidewalk cleaning, security, marketing and economic development, and public improvements such as acquisition and maintenance of parking facilities, benches, trash receptacles, street lighting, decoration and public plazas.	A BID can only fund activities in designated commercial districts. BID formation is generally time and resource intensive and requires the support of a majority of assessees at minimum. BID funds are most effective when leveraged with CDBG funds and redevelopment funds.

Source	Agency	Description	Applicability	Limitations
Economic Development cont'd				
Community Development Block Grants (CDBG)	Dept. of Housing and Urban Development (HUD), Camarillo Dept. of Community Development	Provides grant funds for activities such as public works; rehabilitation loans and grants; land acquisition, demolition, and relocation for redevelopment; public services; and affordable housing, social services and projects for the elderly or disabled. CDBG-funded projects and activities must principally benefit low and moderate-income persons, aid in the prevention or elimination of blight, or address an urgent need. CDBG funds have provided a limited source of revenue for Camarillo.	The City has an active CDBG program and takes applications annually for projects to fund. Applicants are typically private non profit or for-profit organizations that provide development or social services. CDBG funds could be used to renovate deteriorated structures.	Most CDBG-funded activities target residential neighborhoods rather than commercial areas. Dawson Drive must compete with projects throughout the City.
Community Development Block Grants (CDBG) Section 108 Loan Guarantees	Dept. of Housing and Urban Development (HUD)	Provides a guaranteed loan commitment secured through CDBG funds. Provides communities with a source of financing for economic development, housing rehabilitation, public facilities, streetscape and large scale physical development projects. All projects and activities must either principally benefit low and moderate-income persons, aid in the elimination or prevention of slums and blight, or meet urgent needs of the community. Maximum repayment period for Section 108 loan is 20 years.	Section 108 loan guarantees could assist with a wide range of economic development activities and streetscape improvements in the Dawson Drive areas.	Not a new source of funds, as City must pledge future CDBG funds as a guarantee on the loan repayment. Typically Section 108 loan would be repaid by revenues generated by a proposed project or redevelopment tax increment.

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Source	Agency	Description	Applicability	Limitations
Economic Development cont'd				
Brownfield Economic Development Initiative (BEDI)	Dept. of Housing and Urban Development (HUD)	Provides grants to help local governments redevelop brownfields, such as abandoned, idled, or underutilized properties, including industrial and commercial facilities where expansion or redevelopment is complicated by the possible presence of environmental contamination. Grant must be linked with a new Section 108-guaranteed loan commitment secured by the City's CDBG funds. A local government may re-loan the Section 108 loan proceeds and provide BEDI funds to a business or other public entity eligible to carry out a specific approved brownfields economic development project, or the public entity may carry out the eligible project itself. In either case, BEDI grant funds and the Section 108 proceeds must be used to support the same eligible BEDI project.	BEDI funds could assist in the remediation of sites, making them economically viable to develop.	Must be linked with CDBG and Section 108. Scope of environmental contamination on Dawson Drive properties is unknown.
Brownfields Cleanup Revolving Loan Fund (BCRLF)	Environmental Protection Agency (EPA)	Loan program to provide financial assistance for the remediation of brownfields. Enables state and local governments to make low interest loans to carry out cleanup activities on properties that have an actual release or substantial threat of release of a hazardous substance that threatens public health or welfare.	BCRLF funds could assist funding site preparation and development activities.	Scope of environmental contamination on Dawson Drive properties is unknown.
SBA Business Assistance Programs	Small Business Administration (SBA)	Provides a range of programs from small business loans to equity investment programs tailored to small business needs. Loans programs include Basic 7(1) Loan Guaranty, Certified Development Company (CDC), and Microloan and Loan Prequalification. SBA's investment program consists of privately owned and managed investment firms that provide venture capital and start-up financing to small businesses. Generally, technical assistance is provided, but grants and loans are also available.	SBA funds could help strengthen the economic base of the business community. Eligible activities include one on one counseling with small business owners, hosting workshops, classes, and web site design.	Technical assistance is more common than direct loans or grants, so only limited new funding is available.

Source	Agency	Description	Applicability	Limitations
Neighborhood Enhancement				
Infill Infrastructure Grant Program (IIG)	California Housing and Community Development (HCD)	Provides grants to assist in the new construction and rehabilitation of infrastructure that supports higher-density affordable and mixed-income housing in locations designated as infill.	IIG funds could finance infrastructure necessary for housing development in the Dawson Drive area.	Long term funding availability is uncertain.
Transit-Oriented Development (TOD)	California Housing and Community Development (HCD)	Provides loans for rental housing development and land acquisition for proposed housing development. Grants for infrastructure that supports housing or facilitates connectivity to transit from one or more specific housing developments. Mortgage assistance for first-time low or moderate-income homebuyers.	Metrolink station and related transit opportunities may make projects competitive to receive state TOD funds.	Long term funding availability is uncertain.
Low Income Housing Tax Credits (LIHTC)	California Tax Credit Allocation Committee (CTCAC)	Allocates federal and state tax credits to encourage private investment in affordable rental housing for households meeting certain income requirements.	LIHTCs could finance affordable housing development.	Application process is competitive and complicated.

Source: City of Camarillo, Seifel Consulting Inc.

APPENDIX **B**

ADOPTION RESOLUTION

RESOLUTION NO. 2010-48

A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF CAMARILLO APPROVING THE CONCEPTS AND DESIGN GUIDELINES FOR THE APPROXIMATELY 180-ACRE AREA SOUTH OF THE U.S. 101/VENTURA FREEWAY, EAST OF THE UNION PACIFIC'S MAIN LINE, NORTH OF PLEASANT VALLEY ROAD, ALONG THE EAST AND WEST SIDES OF LEWIS ROAD, FURTHER DESCRIBED AND SET FORTH AS THE DAWSON DRIVE AREA

The City Council of the City of Camarillo resolves as follows:

SECTION 1: The City Council finds and declares as follows:

A. The City of Camarillo has reviewed the Dawson Drive Area Concepts and Design Guidelines.

B. In January 2009, the City Council authorized the consulting firm of Mogavero Notestine Associates of Sacramento, California, to compile a series of concepts and design guidelines for the Dawson Drive Industrial Area.

C. The guidelines are intended to articulate a vision for the revitalization of this area and establish a set of design standards and improvements that will increase land use flexibility and guide its redevelopment into a vibrant mixed-use district, and establish a framework for development and redevelopment within the area.

SECTION 2: The City Council deems the Dawson Drive Area Concepts and Design Guidelines reasonable based on the following findings:

A. The draft Dawson Drive Area Concepts and Design Guidelines document is consistent with the goals and objectives of the City of Camarillo's General Plan and the Camarillo Corridor Redevelopment Project. The document provides a framework for future improvements and elimination of blight within the 180-acre Dawson Drive area to accommodate the enhancement of circulation, place-making and redevelopment. The document will function as a Specific Plan to provide further guidance for the development of the area.

B. Mitigated Negative Declaration (MND 2010-1) was prepared and circulated for adoption of the draft document. The MND concluded that all environmental impacts associated with adoption of the Guidelines can be mitigated. The Guidelines, as prepared, will foster an overall improvement of the environment for the 180-acre area.

C. The site is generally adequately served by streets and utilities of sufficient size and capacity to accommodate the improvements as contained within the draft document. Future improvements identified for the area will be accomplished as project development requirements and capital improvements.

SECTION 3: The City Council approves Dawson Drive Area Concepts and Design Guidelines. The record of proceedings upon which this decision is based, is located in the Department of Community Development, which is the office of record for the same.

PASSED AND ADOPTED on May 12, 2010.

Kevin Kildee
Mayor

ATTEST:

Jeff Madland
City Clerk

I, Jeffrie Madland, City Clerk of the City of Camarillo, certify Resolution No. 2010-48 was adopted by the City Council of the City of Camarillo at a regular meeting held May 12, 2010, by the following vote:

- AYES: Councilmembers: Craven, McDonald, Morgan, Waunch, Mayor Kildee
- NOES: Councilmembers: None
- ABSENT: Councilmembers: None

Jeff Madland
City Clerk

Copy: Community Development
General Services
Mogavero Notestine Assocs.